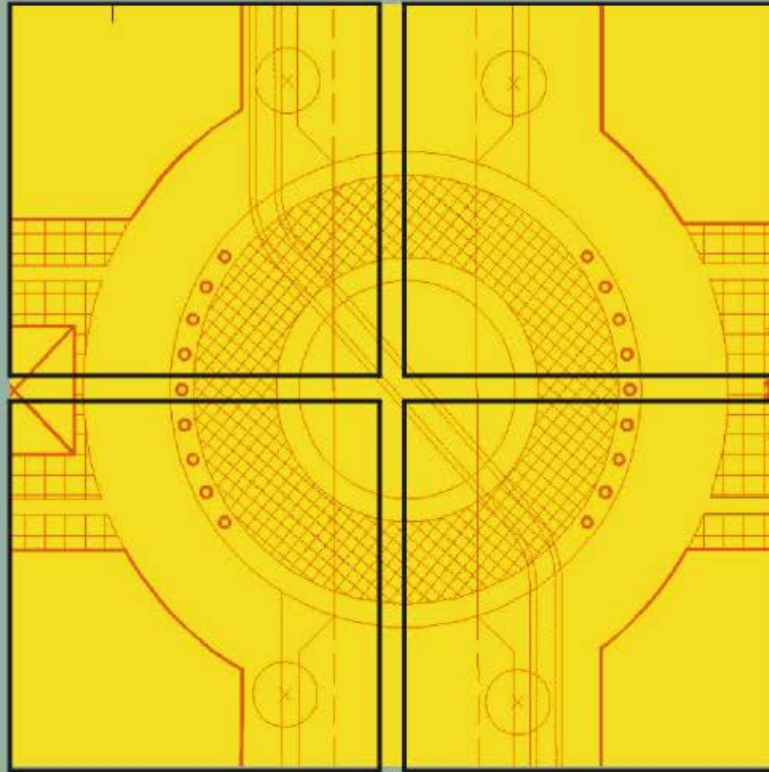


A glimpse of the past....for the future



CLOVERDALE TOWN CENTRE



SURREY
CITY OF PARKS

Planning & Development Department

**Land Use &
Urban Design**

October 30, 2000

CLOVERDALE TOWN CENTRE

LAND USE

and

URBAN DESIGN
CONCEPT PLAN

October 30, 2000

ACKNOWLEDGEMENTS

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CLOVERDALE LAND USE PLAN AND URBAN DESIGN CONCEPT

I. INTRODUCTION

This comprehensive planning and design document provides land development, circulation, open space and other policies in relation to the Cloverdale Town Centre. It also establishes land use and density patterns, vehicular and pedestrian circulation concepts and open space systems. The urban design component of the Plan will provide directions to developers and the general public to achieve the community-wide objectives articulated through the planning process and to which a majority of the stakeholders in Cloverdale have agreed.

II. STUDY AREA

A. The Cloverdale Context

Cloverdale is a growing community of about 25,000 people located on the east side of Surrey adjacent to the Township/City of Langley. Cloverdale has a very rich historical past due to its prominence in agriculture and association with the railway. The area will be experiencing significant residential growth due to emerging new neighbourhoods in North and West Cloverdale and Clayton. By the year 2021, there could be as many as 70,000 people living in the general Cloverdale area.

Despite population growth in the area, growth in the commercial sector of the Town Centre is not keeping pace. This is likely due to the strong competition of the Willowbrook Regional Shopping Centre and nearby commercial centres. As a result, the Town Centre is experiencing some difficulties in capturing businesses and maintaining its vitality.

B. The Cloverdale Town Centre

The Plan area (or the Study area) includes the Cloverdale Town Centre area as shown on Map No. 1. Surrey's Official Community Plan designates lands within the Town Centre as Town Centre, Commercial and Multiple Residential (also shown on Map No. 1). The Cloverdale Town Centre area encompasses about 195 acres (80 hectares).

It is strategically located at the crossroads of two major highways:

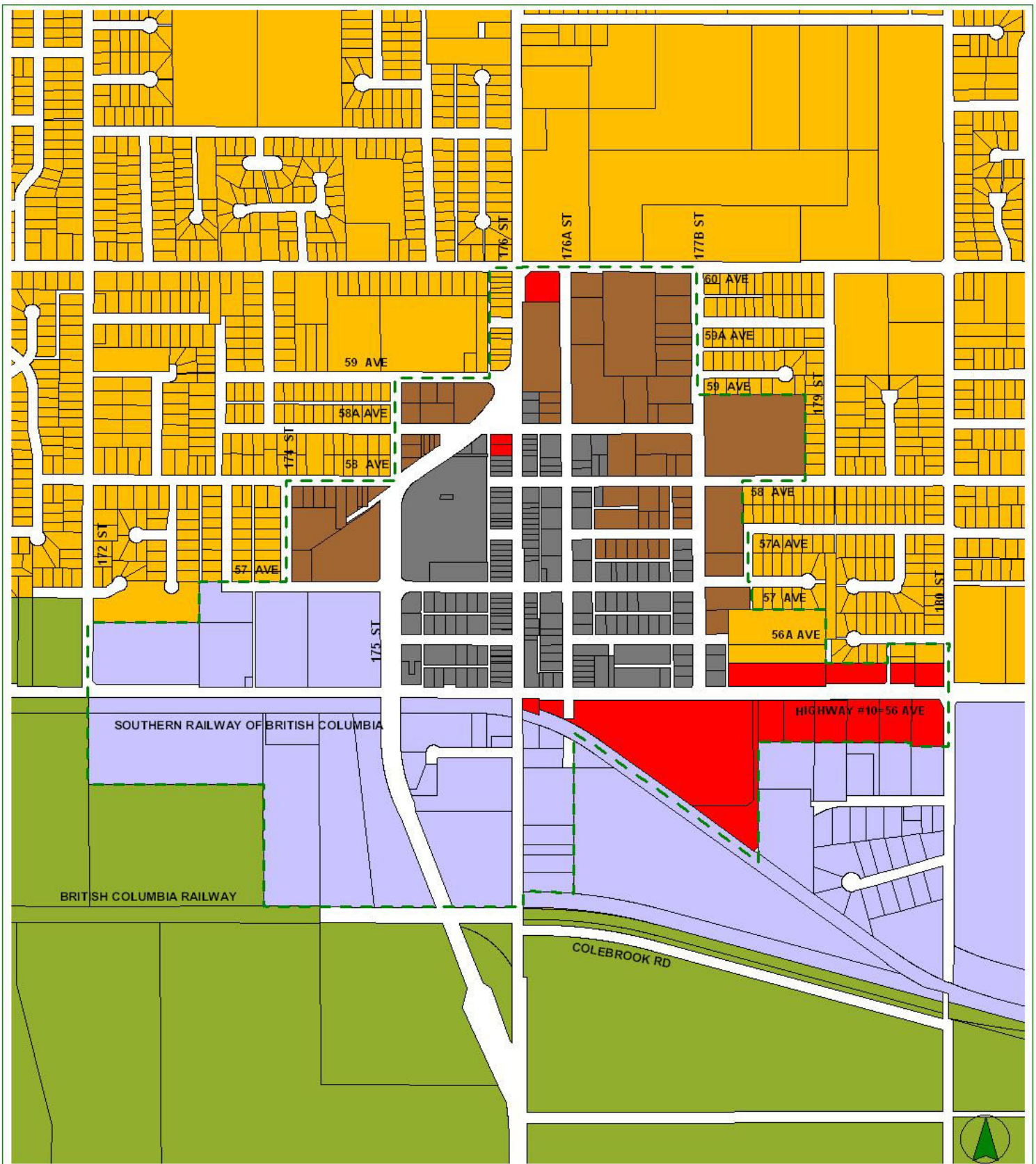
- Highway No. 10 is located immediately south of the historical Town Centre and is the main approach and access from the east and the west; land uses along this corridor include industrial, highway commercial, tourist-related commercial and a newly redeveloped shopping centre on the south side of the highway south of the historic Town Centre.
- Highway No. 15 (Pacific Highway) is the main north-south access to the Town Centre, and serves as a bypass for through traffic along the western edge of the Town Centre; this highway is elevated over the industrial lands and B.C. Hydro rail tracks south of Highway No. 10.







Land fronting the major approaches to the Town Centre is included in this review along with some commercial and industrial lands on the south side of Highway No. 10.

III. TERMS OF REFERENCE FOR THE CLOVERDALE TOWN CENTRE STUDY

The Terms of Reference for this study were approved by City Council in May, 1998. The Terms of Reference outlined a strategic planning process that defined the study area, the plan preparation process and the content of the Plan. The objectives of this study are:

1. To review the land uses in the Cloverdale Town Centre;
2. To prepare a comprehensive land use plan to support the formation of a strong identity, renewed vitality and enhanced economic base for the Cloverdale Town Centre;
3. To develop an Urban Design Concept and related design guidelines to maintain and enhance the character of the Town Centre within the context of the proposed land uses and the larger Cloverdale area; and
4. To establish development strategies that will revitalize the Town Centre and therefore enable it to successfully attract businesses, tourists and local residents and to enable it to prosper and compete with nearby commercial areas.



- | | | |
|--|---|---|
|  TOWN CENTRE (TC) |  INDUSTRIAL (IND) |  URBAN RESIDENTIAL (URB) |
|  COMMERCIAL (COM) |  MULTIPLE RESIDENTIAL (RM) |  AGRICULTURAL (AGR) |

O.C.P. Land Use Designation

Map No. 1

IV. THE PLANNING PROCESS

A. Overview

Since City Council's approval of the planning process in May, 1998, the following strategic steps have taken place in formulating a new vision for the Cloverdale Town Centre:

1. Identifying area issues, planning opportunities and constraints with special attention to the economic factors affecting the Town Centre;
2. Setting planning goals and objectives;
3. Generating planning options;
4. Selecting a preferred option; and
5. Formulating a comprehensive land use plan, urban design concept and development strategy.

All of these steps occurred within the context of a collaborative approach with the community.

Background research was undertaken with respect to the local economy, land use, local history and community development. An analysis of the commercial development prospects for the Town Centre was undertaken, and the character and architecture of Cloverdale were assessed.

At various milestones throughout the process, reports and recommendations were presented to City Council about the progress of the plan and the various design initiatives. Following the selection of a preferred option, a three dimensional model was constructed to illustrate how the future Town Centre may look if the proposed plan is followed.

B. Cloverdale Town Centre Focus Group

A Cloverdale Town Centre Focus Group was formed comprised of 18 individuals including residents, business owners and stakeholder group representatives. The Focus Group meetings were instrumental in developing land use options, understanding the local heritage and formulating an urban design concept. The Focus Group was invaluable in bringing local knowledge and business expertise into the planning process.

C. Public Meetings

A number of public meetings were held throughout the planning process. The first Public Information Meeting was held on June 18, 1998 to introduce the planning process to the community, gather ideas, identify issues and set objectives. Based on the questionnaires and comments received, the most significant findings were:

1. The main positive characteristic is Cloverdale's small town character and local identity;
2. The main negative characteristic is the rundown image;
3. There is a need for a better mix of uses in the centre of Cloverdale, including residential and recreational uses; and
4. Local heritage is important to Cloverdale's character; there is a need for urban design guidelines to focus on the local historical character, and to create attractive buildings, introduce more landscaping, form a favourable pedestrian environment and create a welcoming atmosphere.

A second Public Information Meeting was held on February 18, 1999 and approximately 200 people attended to review and comment on three land use options and the proposed urban design concept. Input from this meeting was used to generate a preferred land use plan and urban design guidelines.

A final Public Information Meeting was held on September 14, 2000 at which the public viewed the Cloverdale Town Centre Model and had opportunities for final comments and input into the preferred land use plan and urban design concept. About 150 people attended this meeting and the input received was used to refine and finalize the plan for City Council's approval.

D. Inter-Departmental and Inter-Agency Input

Liaison was maintained throughout the planning process with the Heritage Advisory Commission, the Cloverdale business community, the Engineering Department and the Parks, Recreation & Culture Department. Planning staff also sought input from external agencies (i.e., Ministry of Transportation and Highways) on issues and constraints facing Cloverdale from all perspectives.

E. Selection of the Preferred Land Use Plan

Using initial input from the public and business groups in Cloverdale, the Planning & Development Department prepared three land use options for the Cloverdale Town Centre. These options were presented to the various stakeholders, including business operators and an economic consultant, for review, analysis and comment. The land use options outlined three possible roles for the Cloverdale Town Centre based upon its relative position in the current and future market area: as a small Village Centre, large scale Town Centre, or mid-sized Town Centre. Each of these options included varying amounts of additional residential in the Town Centre along with differing degrees of highway commercial opportunities along Highway No. 10.

After considerable public discourse and economic analysis, the “mid-sized Town Centre option” (known throughout the process as Option 3) was selected, determined to be the most commercially viable and therefore further refined. It subsequently served as the basis upon which the Urban Design Concept, circulation and linkage concepts and the overall enhanced image for the Town Centre was formulated, as articulated in this report.

F. The Cloverdale Business Improvement Area (BIA)

1. Mandate

The Cloverdale Business Improvement Area (BIA) was originally established in 1995 (by City of Surrey By-law) to improve and promote business in the area. The BIA is funded through a rate levied against properties in the Cloverdale Town Centre area, collected in conjunction with annual municipal taxation.

With the mandate to promote goods and services offered by Cloverdale businesses and to increase market opportunities for the commercial district as a whole, the BIA’s objective is to build on Cloverdale’s unique intrinsic qualities and heritage, and to improve the area’s physical, social and economic well-being.

2. Role of the BIA in the Planning Process and Revitalization

The BIA secured financing under the British Columbia Neighbourhood Enhancement Program to develop a Downtown Cloverdale Revitalization Strategy. The BIA held a workshop in early 1999 entitled "Creating a

Consensus Vision of the Downtown Cloverdale in the Year 2009". Objectives formulated from this workshop were: to consult with the community within a BIA driven grassroots process; to view growth as an economic engine to take the community to the 2009 vision; and to provide input into the planning process for the Cloverdale Town Centre.

V. ISSUES, OPPORTUNITIES AND CONSTRAINTS

A. Issues Affecting the Town Centre

At the outset of the planning process, there were several issues facing the Town Centre that constituted the focus of the land use and urban design planning process. These issues are summarized below.

1. *Market:* There is tremendous retail competition with other commercial centres in the area (e.g., Willowbrook) and a perceived lack of variety of consumer goods;
2. *Visibility:* The Town Centre core is not visible from main road corridors and directional/gateway signage is poor;
3. *Traffic and Safety:* Two highways, which have high volumes of truck traffic, border the Town Centre and traffic speed and inadequate turning lanes create difficult access to the Town Centre;
4. *Identity, Uniqueness and Image:* The Town Centre has a confusing identity and image (i.e., is it heritage or western?), and lacks visual interest, coordination, colour and animation;
5. *Development Pattern:* The current commercial built form is spread out, uncoordinated and lacks focus and organization; and
6. *Parking:* There is not enough parking located close to the shopping routes.

B. Opportunities and Constraints

The Cloverdale Town Centre presents some opportunities and constraints which are addressed in the Plan. These are:

1. Opportunities

- a) The Fairgrounds north of the Town Centre is a significant land use which could potentially complement the commercial activities of the core commercial area;
- b) The agricultural and industrial lands to the south of Highway No. 10 provide an economic base for business development to reinforce the activities of the core commercial area;

- c) The Town Centre has a rich history and emerging character that can be drawn upon and enhanced; and
- d) The City currently owns a number of properties in the Town Centre that have the potential to contribute in a significant way to any redevelopment schemes.

2. Constraints

- a) Direct access to the core commercial area from the existing residential neighbourhood to the west is restricted by Highway No. 15 (Pacific Highway);
- b) New residential areas in North Cloverdale are not strongly connected with the Cloverdale Town Centre area; and
- c) The residential areas immediately to the east of the Town Centre core are restricted by the discontinuity of the street grid pattern and need better linkages to the commercial core.

VI. ECONOMIC STUDY AND RECOMMENDATIONS

A. Purpose of Study

In November, 1999 Coriolis Consulting Corp. completed an economic assessment of the Cloverdale business area to assist the City in formulating land use options and strategies. The Coriolis group analysed the Town Centre's prospects for future commercial development, identified strategies that could increase its commercial growth potential, and suggested actions that will help revitalize the existing commercial core by attracting new businesses, increasing sales and generally improving the character of the area.

B. Conclusions

The findings of the Economic Study¹ are summarized below and were incorporated into the final plans and development strategies for the Cloverdale Town Centre.

1. General Findings

- a) Cloverdale's commercial area is large for a community that is close to major commercial centres (such as the Willowbrook Regional Shopping Centre). This is because some of Cloverdale's commercial space pre-dates commercial development in the nearby regional centres and because Cloverdale has a role serving non-resident employees at Cloverdale's industrial areas, tourists and highway travelers.
- b) The Town Centre's development potential is highly dependent on the overall rate of population growth in the trade area and whether or not another large shopping centre is approved outside the Town Centre.
- c) It is estimated that Cloverdale currently has about 61,000 square metres (656,000 square feet) of retail/service floor space and about 17,000 square metres (181,000 square feet) of office floor space.
- d) The Consultant predicts that by 2006, under a low growth scenario, the Cloverdale trade area could support about 20,400 square metres (220,000 square feet) of new retail/service floor space and 6,000 square metres (64,000 square feet) of new office floor space. Under a high growth scenario, the Cloverdale trade area could support

¹ All of the information in this Section is from: "Commercial Development Prospects for the Cloverdale Town Centre", Coriolis Consulting Corp., November, 1999.

about 35,500 square metres (382,000 square feet) of new retail/service floor space and about 10,400 square metres (112,000 square feet) of new office floor space. However, if a new shopping center is approved outside of the Town Centre, the supportable commercial floor space in the Town Centre would be reduced and its competitiveness in the retail market would be weakened.

2. Summary of the Distribution of Commercial Floor Space

Tables No. 1 and No. 2 compare the distribution of supportable new commercial floor space in the Cloverdale Town Centre trade area under two scenarios for low and high population growth projections.

| Low Population Growth | Scenario No. 1 <i>(no new shopping centre outside of the Town Centre)</i> | Scenario No. 2 <i>(new shopping centre built outside of the Town Centre)</i> |
|--|--|---|
| Total new retail and service floor space in the primary trade area | 220,000 sq. ft. | 220,000 sq. ft. |
| New shopping centre outside Town Centre | n/a | 150,000 sq ft. |
| Scattered in primary trade area | 30,000 sq. ft. (3 plazas) | 30,000 sq. ft. (3 plazas) |
| Town Centre | 190,000 sq. ft. | 40,000 sq. ft. |
| Total new office floor space in the primary trade area | 64,000 sq. ft. | 64,000 sq. ft. |
| New shopping centre outside Town Centre | n/a | 20,000 sq. ft |
| Town Centre | 64,000 sq. ft. | 44,000 sq. ft. |

Table No. 1 – Low Population Growth (to 2006)

| High Population Growth | Scenario No. 1 <i>(no new shopping centre outside of the Town Centre)</i> | Scenario No. 2 <i>(new shopping centre built outside of the Town Centre)</i> |
|--|--|---|
| Total new retail and service floor space in the primary trade area | 380,000 sq. ft. | 380,000 sq. ft. |
| New shopping centre outside Town Centre | n/a | 150,00 sq. ft. |
| Scattered in primary trade area | 50,000 sq. ft. (5 plazas) | 40,000 sq. ft. (4 plazas) |
| Town Centre | 330,000 sq. ft. | 190,000 sq. ft. |
| Total new office floor space in the primary trade area | 112,000 sq. ft. | 112,000 sq. ft. |
| New shopping centre outside Town Centre | n/a | 20,000 sq. ft. |
| Town Centre | 112,000 sq. ft. | 92,000 sq. ft. |

Table No. 2 - High Population Growth (to 2006)

3. Recommendations

The Coriolis Group recommended several ways by which Town Centre commercial development prospects could be maximized, as outlined below.

- a) *Adopt a favourable commercial land use policy in Cloverdale.*

To maximize the Town Centre's development potential, it is recommended that there should be no new shopping centre approved outside of the Town Centre, although it is appropriate that some small, neighbourhood scale commercial plazas be permitted in surrounding growing residential areas.

b) *Adopt a favourable residential land use policy in Cloverdale.*

Multi-family residential concentrations should be allowed in and near the Town Centre. Multi-family residential concentrations outside of the Town Centre should be in locations that have good access to the Town Centre encouraging people to shop there rather than leave the trade area.

c) *Adopt favourable light industrial land use policy.*

Cloverdale's employment growth potential can be maximized by servicing lands with highway access, encouraging upgrades to east-west access that would benefit industrial lands south of Highway No. 10, and ensuring that zoning regulations are flexible to allow the mix of office, warehouse, sales and service that are typical in business parks. The Town Centre will benefit from business growth in the surrounding area.

d) *Increase the Town Centre's population capacity and attractiveness.*

Residential development should be planned to ensure that the community offers a good mix of civic amenities and facilities, ensuring that municipally-imposed development costs are competitive with other nearby growth areas, maintaining the small block grid road system (which helps create Cloverdale's special ambience), creating quality urban streetscapes including street trees and good sidewalks, and maintaining and extending the lane system which makes streets and street life more attractive.

e) *Draw more visitors and shoppers from outside the trade area.*

Possible initiatives include retaining and enhancing good older buildings because these create the area's basic ambience and create an attractive streetscape, facilitating more special events to give people more of a reason to come to Cloverdale, promoting the nearby amenities and facilities such as Mound Farm, the potential Learning and Discovery Centre, and recruiting the types of businesses such as galleries, gift shops and antique stores that are able to draw visitors. Along with this initiative, Cloverdale business and community groups should undertake and promote year-round events, exhibitions and activities to attract people to the Town Centre rather than relying only on the existing annual fairs and exhibitions.

f) *Reduce leakage of resident spending.*

Cloverdale's proximity to larger retail centres will always impose a ceiling on Cloverdale's potential. A significant portion of local trade area sales potential will inevitably leak out to department stores, big box retailers and specialty stores in these other areas. There are no significant gaps in Cloverdale's business mix relative to other similar commercial districts. In fact, Cloverdale has an untypically large and diverse inventory of retail, service and office businesses, so there is limited potential to reduce leakage. Reducing leakage can mainly be achieved by greater marketing efforts on the part of the individual stores and the whole Town Centre area.

g) *Use the City's land inventory.*

The City of Surrey owns lands in and near the Town Centre. The use of the lands could contribute to enhancing the Town Centre's commercial prospects. Generally, all City lands within the Town Centre area are good candidates for commercial or multi-family residential development. The site of the Cloverdale Mall has significant potential for expansion and is in a good location for more intensive commercial uses, which should be encouraged.

VII. LAND USE PLAN AND POLICIES

A. Land Use Planning Objectives and Principles

To achieve the objectives of the Study (see Section III), the land use planning objectives are focused on developing a strong and healthy Town Centre by concentrating the commercial activities within the Town Centre core and limiting the expansion of auto-oriented uses along the highways. The goal is to provide for the Cloverdale community a sense of place and an environment that is attractive, pedestrian-friendly and unique in character for shopping, services, and entertainment.

The following planning and land use principles guided the development of the proposed Land Use Plan:

1. Recognize the aspirations of the Cloverdale community in striving for a family-oriented, pedestrian-friendly Town Centre that is characterized by its place in history.
2. Reinforce the identity of the Town Centre core by defining its boundaries with features such as landmarks and gateways; and by creating a special character or theme along its main streets.
3. Identify potential development opportunities and appropriate land uses that could significantly enhance the viability of the Town Centre and achieve the community's objectives.
4. Develop a pedestrian-oriented land use pattern that would support and facilitate the development of an inviting and attractive streetscape.
5. Provide land use opportunities for a range of commercial uses that would offer a wide selection of goods and services within the Town Centre.
6. Strive for a land use pattern that would allow an increase in the residential population located within easy walking distance of the Town Centre.
7. Provide land use opportunities for mixed commercial and residential projects within the Town Centre core.
8. To develop land use options that would contain highway and service commercial uses along the highways in order to ensure that a strong commercial base flourishes in the commercial core of the Town Centre.

9. Provide recreational and community services within or near the commercial core, and ensure that these public amenities appeal and attract people to the Town Centre area.
10. Strive for safe and direct vehicular access to the Town Centre from the main highways and from the residential community.
11. Provide convenient public parking facilities within easy walking distance of the Town Centre core.
12. Ensure that pedestrian access to the Town Centre is easy, safe and inviting for the local residents.

B. Overview of the Proposed Land Use Plan (Map No. 2)

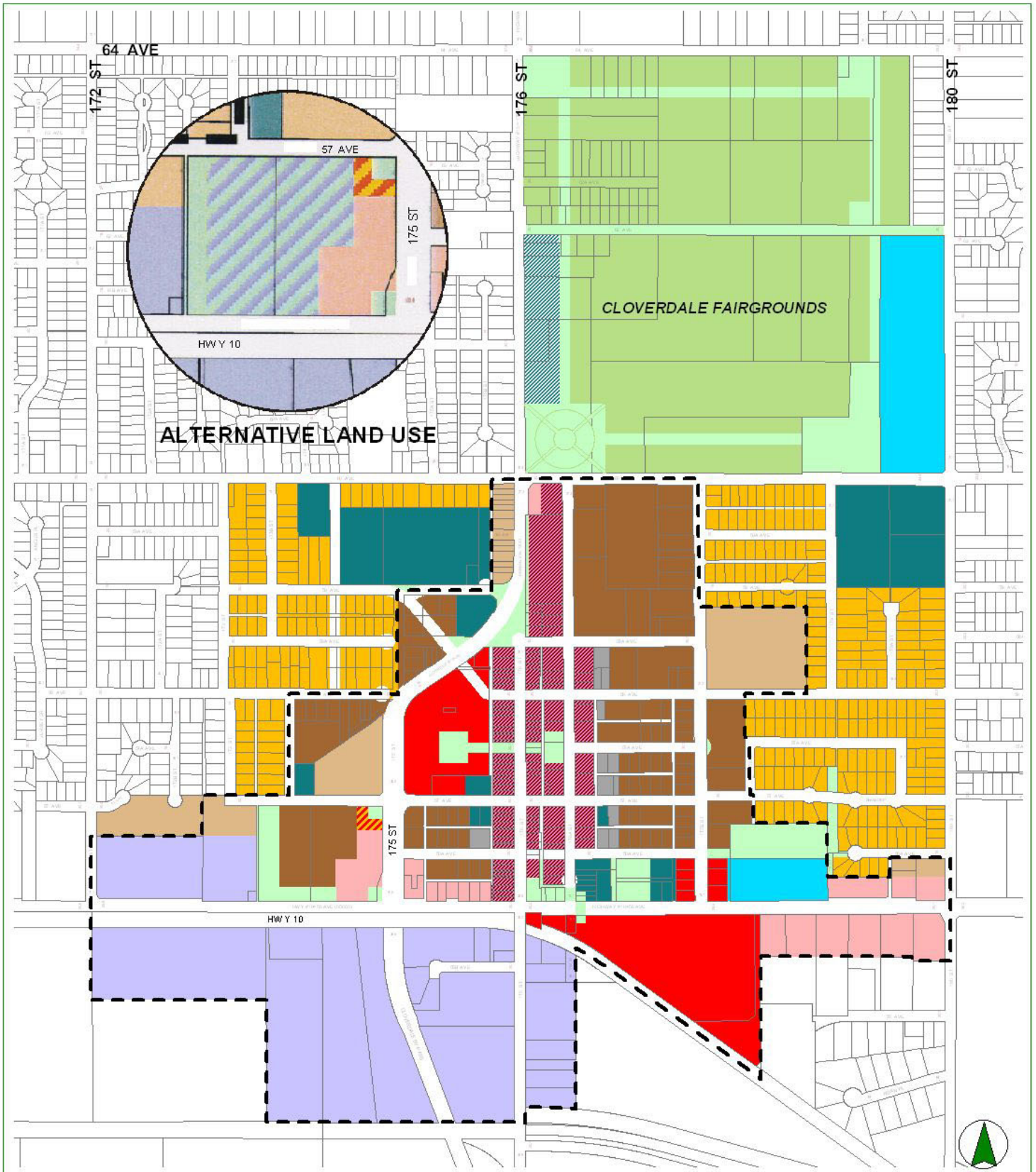
The final Land Use Plan is shown on Map No. 2 (Cloverdale Town Centre Land Use Plan). The development of the proposed Land Use Plan was based upon the following three initiatives:

1. Community goals and aspirations and guiding principles to achieve community objectives;
2. Input from the public questionnaires which called for Cloverdale's characteristics to have a town focus, local identity and heritage feel; and
3. The findings of the Economic Study which assisted in determining the appropriate land uses and the strategies required to achieve a healthy and viable Town Centre.

The main features of the Land Use Plan are outlined below.

1. The focus of the Town Centre is along the two main streets: 176 and 176A Streets.
2. There are limited expansion opportunities for service and highway commercial uses along Highway No. 10.
3. There is a strong commercial link between the Town Centre core and the Cloverdale Fairgrounds.
4. The redevelopment of the City's Cloverdale Mall site is an integral part of revitalizing the Town Centre.
5. There is safe pedestrian access and linkage from the Town Centre to the Clover Square Village site (south of Highway No. 10).

6. Development of a new intersection is proposed at 58 Avenue and Highway No. 15 to substantially improve accessibility and safe pedestrian crossing to the Town Centre from the residential areas to the west.
7. City lands located east of the Library and Senior's Centre will be developed for public use.
8. The BCBC works yard site at Highway No.'s 10 and 15 will be redeveloped to provide more attractive land uses including residential, commercial, business park and gateway features.
9. An integrated parks and open space system will provide good linkages to the residential community.
10. Convenient public parking facilities will be provided within easy walking distance of the Town Centre.



- | | | | |
|---------------------------|----------------------------|--------------------|------------------------|
| TOWN CENTRE COMMERCIAL | MEDIUM DENSITY RESIDENTIAL | INSTITUTIONAL | SCHOOL |
| RETAIL/SERVICE COMMERCIAL | TOWNHOUSES | COMMUNITY SERVICES | CLOVERDALE FAIRGROUNDS |
| RESIDENTIAL/COMMERCIAL | INDUSTRIAL | PARKING | STUDY BOUNDARY |
| HIGHWAY COMMERCIAL | INDUSTRIAL BUSINESS PARK | PARKS/OPEN SPACE | |

C. Commercial Component of the Plan

1. Town Centre Core Commercial

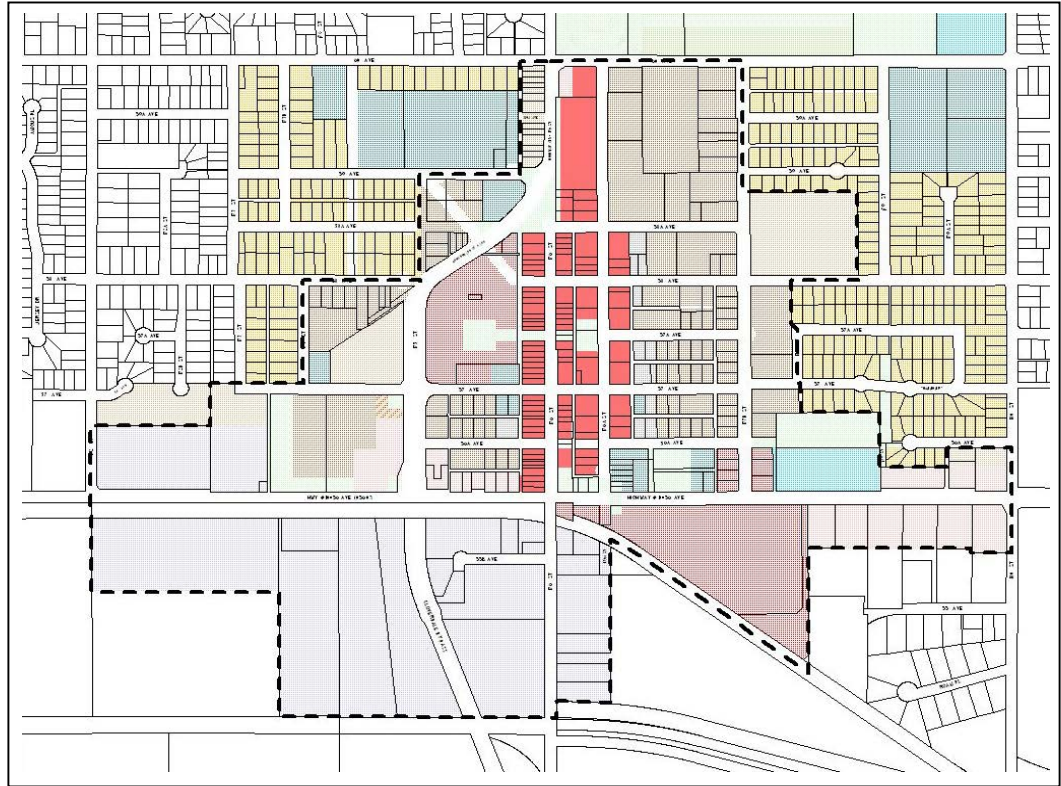


Figure No. 1 – Town Centre Commercial

The Town Centre commercial core is located along 176 and 176A Streets from Highway No. 10 to 60 Avenue (see Figure No. 1). The strip of land along the west side of 176A Street between 58A and 60 Avenues should be developed for commercial uses and will be the key link in providing continuous commercial interest and activities from the Town Centre core to the Fairgrounds site to the north.

The Town Centre commercial area should include a variety of retail shops, restaurants, businesses and services. Mixed commercial and residential developments are encouraged to integrate a residential component with the street-level commercial to promote evening activities and interest.

2. Retail/Service Commercial Uses

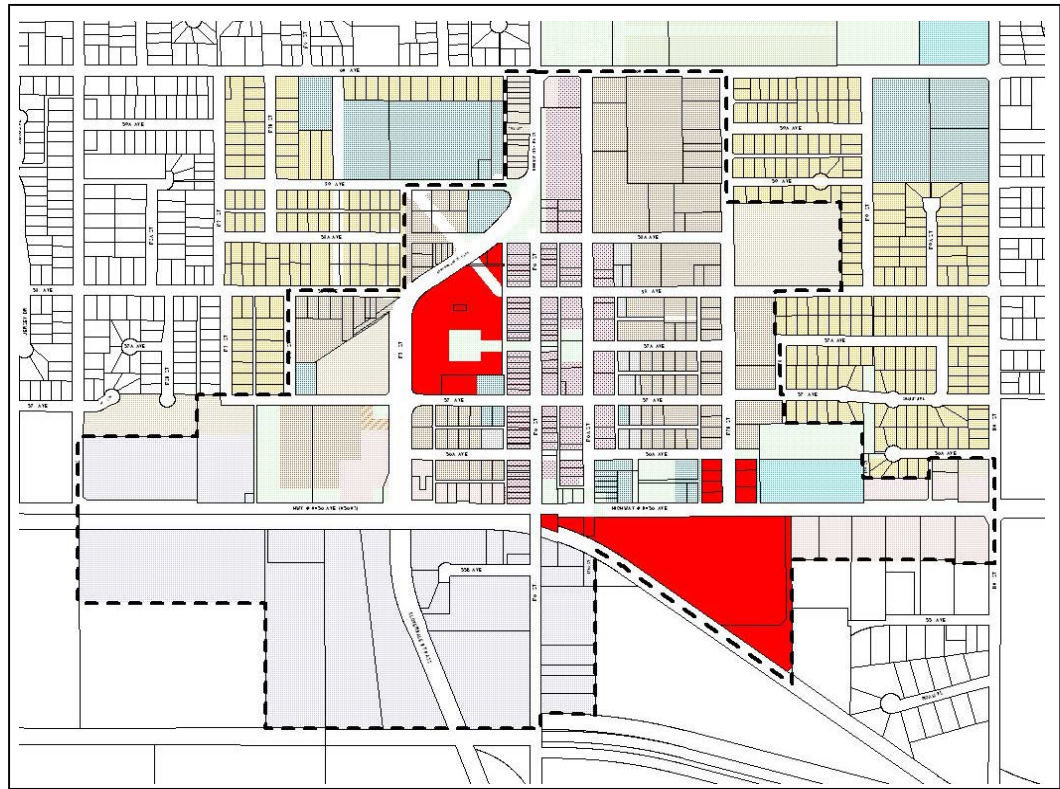


Figure No. 2 – Retail/Service Commercial

The locations of retail and service commercial areas are shown on Figure No. 2. Retail and business uses requiring a large floor plate, such as major food outlets, large chain stores and office supplies and home furnishing stores should be located at the edge of the Town Centre adjacent to the commercial core or along major transportation corridors.

The Plan identifies two major shopping centre sites for this purpose: the existing Clover Square Village shopping centre on the south side of Highway No. 10 at 177B Street, and the City's Cloverdale Mall site north of 57 Avenue at Highway No. 15. The development of these two sites should contain these larger commercial uses and should complement rather than compete with the smaller retail outlets located within the Town Centre core. The combination of the two types of commercial uses and services offered by the two shopping centres and the smaller Town Centre retail outlets can provide the Cloverdale community with the necessary wide selection of goods and services.

In addition to the two shopping centre areas is a small pocket of existing retail/service commercial uses along 177B Street to the north of Highway No. 10. Although this pocket of existing commercial is located away from

the Town Centre core, it can remain to provide convenient services to the adjacent residential area.

3. Mixed Commercial and Residential Uses

One of the recommendations of the Economic Study is to add more resident population in and near the Town Centre as a strategy for enhancing commercial development potential. Residents living within the Town Centre area will likely shop locally because of the convenience and access to service and retail outlets by foot. Allowing and encouraging a mix of residential and commercial uses within the Town Centre area will therefore, enhance the viability of the Town Centre. The Land Use Plan encourages a mix of residential and commercial uses within the commercial core, particularly along 176 and 176A Streets.

4. Highway Commercial Uses



Figure No. 3 – Highway Commercial

Highway commercial uses should be limited to certain lands fronting Highway No. 10 between 175 Street (Highway No. 15) and 180 Street (see Figure No. 3). In order to reinforce the commercial focus at the core,

the Plan recognizes the establishment of these auto-oriented uses along the highway, but strongly discourages their growth and development beyond their current boundaries. The Land Use Plan, however, suggests that the easterly portion of the British Columbia Building Corporation (BCBC – a provincial crown corporation) site located on the north-west corner of Highway No. 10 and No. 15 be developed for tourist-related commercial uses due to its high profile location and the opportunity to provide a landmark feature or gateway for the Cloverdale Town Centre.

5. Public Parking Areas

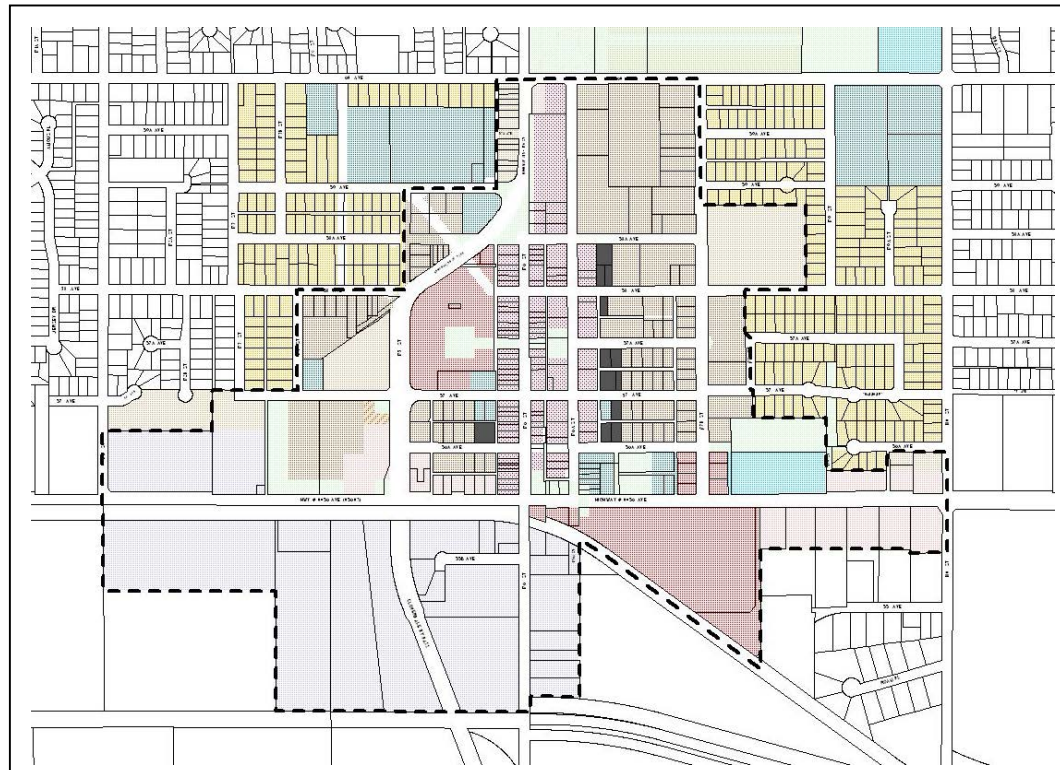


Figure No. 4 – Parking Areas in the Town Centre

The location of proposed public parking areas in the Cloverdale Town Centre are highlighted on Figure No. 4. There are currently four City-owned properties within the Study area: three along 176A Street and one on 56A Avenue, which are to provide off-street parking for the public. These parking facilities were made available to businesses that were unable to provide the required off-street parking on their own properties. The City has granted permits to these merchants to use the City-owned parking spaces as substitution for off-street parking spaces required by the Zoning By-law. These parking spaces are available for free public use. The total parking capacity on the four City lots is approximately 219 spaces.

The proposed Plan calls for the three City parking facilities along 176A Street to be redeveloped for commercial use in order to maintain continuous commercial interest and activities along this street. The total number of parking spaces affected will be approximately 166 spaces and these spaces will need to be replaced at alternative locations.

The Plan has identified the new public parking facilities to be located immediately east of the commercial strip along 176A Street. The intent is to provide convenient public parking located within a short walking distance of the Town Centre core. The proceeds from the sale of City-owned lands on 176A Street should be used to purchase and develop the new parking areas designated in the Plan. The one existing City-owned parking lot on 56A Avenue should remain and continue to be used as a public parking facility.

The Cloverdale Fairgrounds, located immediately north of the Town Centre, has approximately 3,410 parking stalls. Given this high number of parking spaces located close to the Town Centre, some of the spaces should be made available for Town Centre use during off-peak times or during low event days at the Fairgrounds. The City should explore and develop a parking management plan in conjunction with the Fairgrounds with a view to enabling the joint use of this parking facility.

D. Multiple Family Residential Component of the Plan

1. General

The Economic Study emphasized the need to increase the residential population in and around the Town Centre to lend support to its commercial and business growth. This objective can best be achieved by creating opportunities for medium density residential development (i.e., apartments up to 45 units per acre) on lands located immediately adjacent to the commercial core, including those lands that may be perceived to be more attractive for highway and service commercial uses.

The development of medium density residential uses near the Town Centre will allow a higher population to live closer to the commercial area. Multi-family residential developments just outside of the Town Centre area should have good pedestrian and vehicular access to the Town Centre so that the residents can conveniently shop there rather than leave the area by car.

2. Apartments

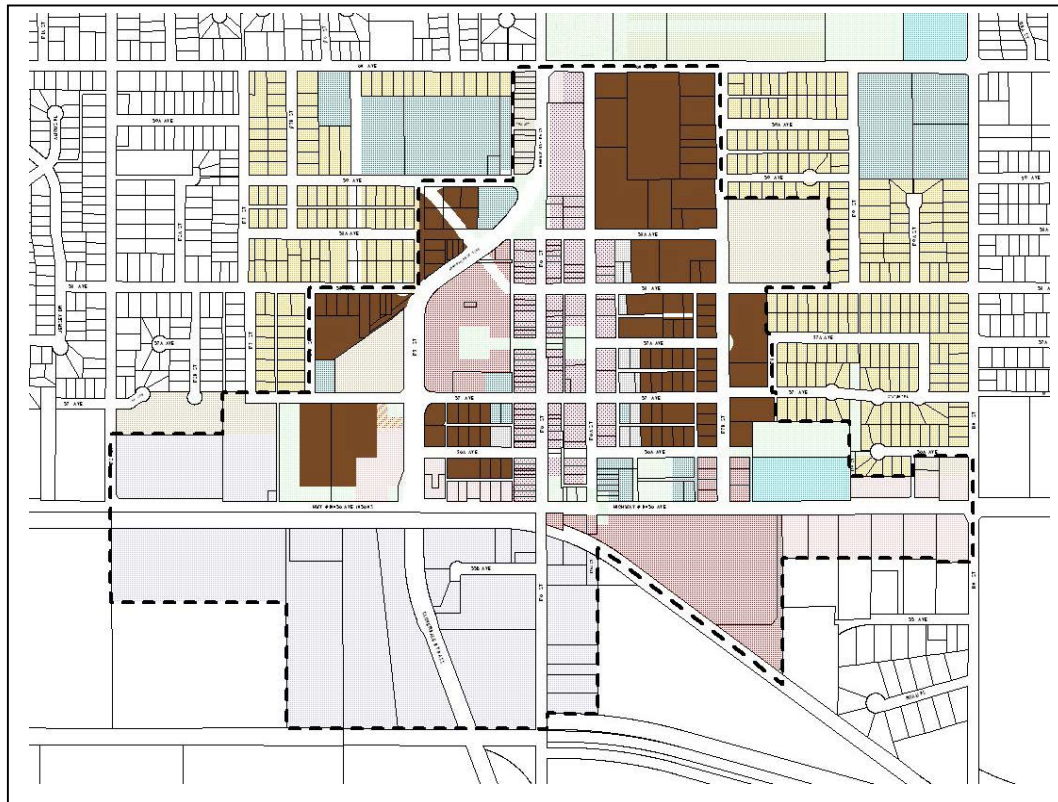


Figure No. 5 - Apartments

Medium density apartments (at a density of up to 45 dwelling units per acre) are encouraged in the areas immediately surrounding the Town Centre core (see Figure No. 5). The apartment design should reflect the character of the Town Centre and the built form may not necessarily take the style of a 3-storey walk-up apartment building. Appropriate apartment sites include lands located between the commercial core and public parking areas from 176A to 177B Streets, 56A to 60 Avenues. On the west side of the Town Centre, the areas along 56A and 57 Avenues east of Highway No. 15 are also encouraged for apartment housing; this could include the possible future redevelopment of the Royal Canadian Legion site at 17567 - 57 Avenue.

The westerly portion of the BCBC site at the northwest corner of Highway No. 10 and Highway No. 15 could be considered for multi-family residential. Due to its large lot size and its location next to high volumes of highway traffic, it would be appropriate for the site to incorporate a comprehensive development scheme to include provisions for adequate buffer and screening along the highways and a design reflecting a mix of housing types and densities from 15 units per acre to 45 units per acre.

Other areas suggested for apartments are located on the west side of Highway No. 15 in the vicinity of 58 and 59 Avenues.

3. Townhouse Development

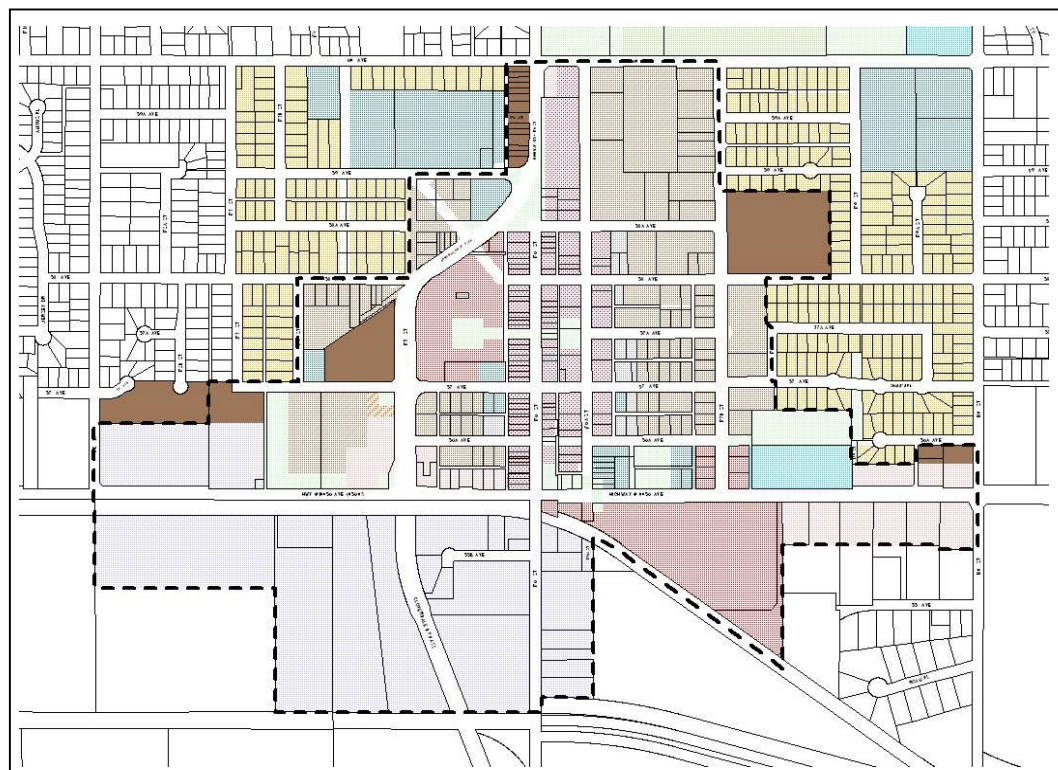


Figure No. 6 – Townhouses

Townhouse developments are an appropriate form of housing in some existing single-family areas near the Town Centre (see Figure No. 6). The townhouse character would serve to provide an acceptable transition to the permanent single-family neighbourhoods in the vicinity. Townhouses at a density ranging from 15 to 30 dwelling units per acre could take the form of row housing or cluster housing along some streets that are currently occupied by detached single-family dwellings. Land assembly to provide a minimum of 2.5-acre sites may be required.

Areas considered appropriate for townhouse development should be located within walking distance of the Town Centre. These areas are designated by the Plan to be at the edge of the Town Centre adjacent to the proposed apartment designations.

4. Redevelopment of Single Family Areas



Figure No. 7 – Single Family Redevelopment Areas

Figure No. 7 highlights the location of existing single-family residential areas within the Town Centre area. While redevelopment of single-family neighbourhoods to townhouses would be the preferred land use, the task of assembling single-family lots for an adequate site area for townhouse development may be difficult to achieve. An alternative option is to subdivide the existing single-family lots to create fee-simple small-lots at a density of approximately 10 to 15 dwelling units per acre. To further

increase the densities in this area, it may be appropriate for some of these lots to also accommodate coach houses. The small-lot form of housing may require lane access. The advantage of this option is that it will allow individual homeowners to develop and thereby increase the density and population in the neighbourhood without the need for land assembly.

Areas suitable for the small-lot type of housing are located just outside the Cloverdale Town Centre and include the existing single family areas on the east side of 173 Street and on the west side of 180 Street up to the proposed apartment designations.

E. Institutional Uses and Community Facilities

1. Schools

The Cloverdale Elementary School is located east of 177B Street on the north side of Highway No. 10. It is an older school built in the mid-1900s to serve the Town Centre and the surrounding residential neighbourhood. While the school location may not be appropriate because it fronts on a very busy highway (Highway No. 10), the school is still in use to serve the Town Centre's residential population. The school has the capacity to accommodate more students, and the School District's short-term plan is to retain the school at this location. In the long term, however, other land use options and development opportunities may be reviewed.

Lord Tweedsmuir Secondary School is located on the border of the Town Centre to the east of the Cloverdale Fairgrounds site north of 60 Avenue and west of 180 Street, and provides secondary education for the Cloverdale community.

The Land Use Plan supports the two existing schools, as they will serve the existing residential population within the Town Centre area. Students from the Town Centre area will attend Cloverdale Elementary from kindergarten to grade 7 and Lord Tweedsmuir Secondary for grades 8 to 12. The proposed changes to the land use and density in the Cloverdale Town Centre may significantly increase the number of students generated by the area depending on the design and marketing of the proposed townhouse and apartment developments in the Town Centre (e.g., whether they are targeted for seniors or families). However, the issue remains as to whether the elementary school site on Highway No. 10 should remain or whether over the long term, the school should be relocated, the land use reviewed and possibly considered for an alternate use. Another concern raised by the School District is the need for a safe and convenient crossing of Highway No. 15 for students living west of the Highway.

2. Recreational Facilities

The majority of the recreational facilities in Cloverdale are located just outside the Town Centre at the Cloverdale Fairgrounds. These facilities are within easy walking distance of the Town Centre and consist of an ice arena and curling facility along 176 Street at 61 Avenue, an outdoor pool at Greenway Park north of 60 Avenue, a baseball facility within the Fairgrounds site, a recently built Youth Park and horseshoe pitching facility along 64 Avenue and an outdoor amphitheatre at the south-east corner of 64 Avenue and 176 Street. The amphitheatre was constructed in late 1999, has a seating capacity of approximately 15,000 to 20,000 people and is an outdoor venue for performances, City events, and general celebrations.

The Parks, Recreation and Culture Department is currently undertaking planning and fund-raising for a Learning and Discovery Centre at the Cloverdale Fairgrounds. The proposed new facility will be between 6,000 and 9,000 square metres (64,600 – 96,900 square feet) in size and will house a new Museum and Archives.

Other future public facilities to be built on the Fairgrounds site may include a multi-purpose community centre, a trade-show centre and all-weather soccer fields.

3. Library

The Cloverdale Library is a modern 930 square metre (10,000 square foot), two-storey building located at the south end of the Town Centre at 5642 - 176A Street adjacent to the Seniors Centre. The building was originally the old Courthouse and Justice Building. It was renovated and opened as a library in October, 1988. Prior to 1988, the Cloverdale Library was located where the Senior's Centre is now. The Cloverdale Library houses one of the largest Canadian genealogy collections in Western Canada.

4. Fire and Police Facilities

Cloverdale Town Centre has a fire hall (No. 8 Fire Hall) located at 17566 - 57 Avenue and a community policing station at 5730 - 176A Street.

F. Parks And Open Space

1. General

Comments received from public questionnaires indicate that there is a desire for more parks, open space and greenery in the Town Centre. This concern arises from the need to improve the Town Centre's appeal for residential development and to create an attractive environment to draw more visitors and shoppers from outside the Cloverdale area. In addressing this need and to improve Cloverdale's physical environment, the Plan proposes the development of public features such as open plazas, urban pocket parks, quality street trees, street furniture, public art, and an inter-connected pedestrian circulation system. The public open spaces are described below and their locations are identified on Map No. 3 (Parks and Open Space). These public urban features integrated with the pedestrian circulation routes will not only improve the quality of the urban environment in and around the Town Centre, but will also provide visual interest for shoppers and visitors to the area.

2. Town Centre Gateways (North and South)

A concern raised by the Cloverdale merchants is that the Town Centre is not visible to traffic, and therefore fails to attract potential visitors and customers from commuting traffic on the two main highways. To address this concern, two open space plazas are proposed: one at the south entrance of the Town Centre at 176 Street and Highway No. 10, and one at the north end of 176 Street at 58A Avenue. The two plazas will be open and provide visual interest at the Town Centre's two main gateways. In order for the plazas to be successful and attract public interest, they should contain special features that are both inviting and attractive. The intent is to draw the attention of traffic along Highways No. 10 and No. 15 to the Town Centre's activities and present an open invitation to visit.

3. Cloverdale Mall Site

The Cloverdale Mall contains approximately 7,900 square metre (85,000 square feet) of retail/office space and is anchored by a 1,950 square metre (21,000-square foot) Safeway store. The Mall is older and somewhat dated and lacks visual interest as well as a wide selection of goods to attract shoppers. Its poor performance could possibly be related to the tenant mix and form of development. It is enclosed and does not

provide the majority of the tenants with direct exposure to traffic along Highway No. 15. The land is owned by the City, but is currently being considered for redevelopment.

The proposed Plan calls for the redevelopment of the mall site to provide a more up-dated shopping centre with direct exposure and visual presence on Highway No. 15. It is important that the development of this site incorporates a pedestrian-friendly open space linkage to the Town Centre core to the east. The open space linkage should be safe, friendly and inviting, and should enable easy direct access to the Town Centre's two main shopping streets. The mall site should also incorporate a public square of sufficient size and shape to function both as a public parking area for the shopping centre as well as a public multi-use area for events (i.e., outdoor farmer's market) and other celebrations at certain times of the year.

4. Hawthorne Square

A small urban plaza is proposed along the west side of 176A Street at 57A Avenue. This plaza could incorporate an historical theme and provide greenery, interest and relief to the shopping activities along 176A Street. The proposed plaza will also serve as an entrance gateway to the new Cloverdale Mall or Shopping Centre and to the gathering place located directly to the west. An open space pedestrian corridor which also extends toward the residential areas to the east will link Hawthorne Square with the new shopping centre.

5. Heritage Garden

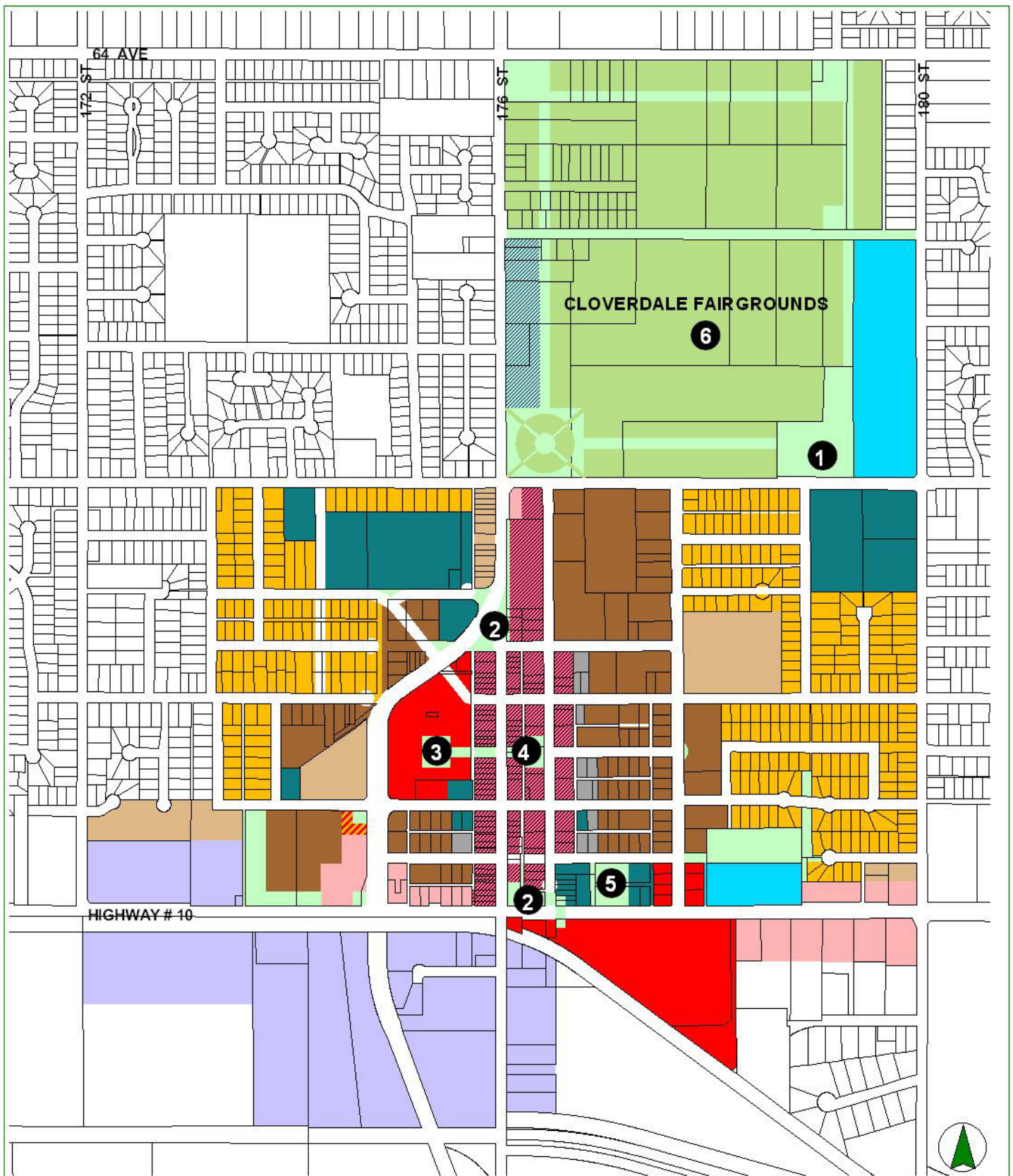
The Cloverdale Library and the Senior's Centre are located at the south end of the Town Centre on 176A Street. To the east of these facilities are vacant City-owned lands that are considered attractive for possible highway commercial uses if not developed for public use. The Plan suggests that these City properties be developed for uses oriented to seniors' interests and activities. Given the exposure along Highway No. 10 and the desire to create an attractive environment at this critical location, one option is to develop a heritage garden (i.e., a rose garden) adjacent to the Senior's Centre.

This heritage garden could stage a multitude of senior's activities that could be operated and managed by seniors. The garden could also be open to the general public for viewing. Suggestions for the proposed heritage garden include a teahouse restaurant, a gazebo of sufficient size

to stage outdoor performances, and a formal rose garden for public viewing. A small area of the site (to the east) could also be set aside to allow the placement of heritage homes that can be relocated here from other areas of Surrey. These homes would provide an historical context to the rose garden setting and may be renovated to permit general office, housing and/or public service uses.

6. Cloverdale Fairgrounds

The Cloverdale Fairgrounds are located directly north of the Town Centre and generate a high level of activity and visitors to the area throughout the year. Because of their close proximity to the Town Centre (and therefore more potential commercial customers), it is important to provide convenient and direct pedestrian access from the Fairground site to the Town Centre. The proposed key link would be an extension of commercial activities along 176A Street from 58A to 60 Avenue. In order to provide an interesting and inviting streetscape, commercial developments along this section should incorporate a double row of trees, wider sidewalks and street furniture to make the street and street life more inviting.



1 GREENAWAY PARK

4 HAWTHORNE SQUARE/PASEO

5 HERITAGE GARDENS

2 NORTH/SOUTH GATEWAYS

3 CLOVERDALE MALL

6 CLOVERDALE FAIRGROUNDS

Parks and Open Space

Map No. 3

G. Industrial

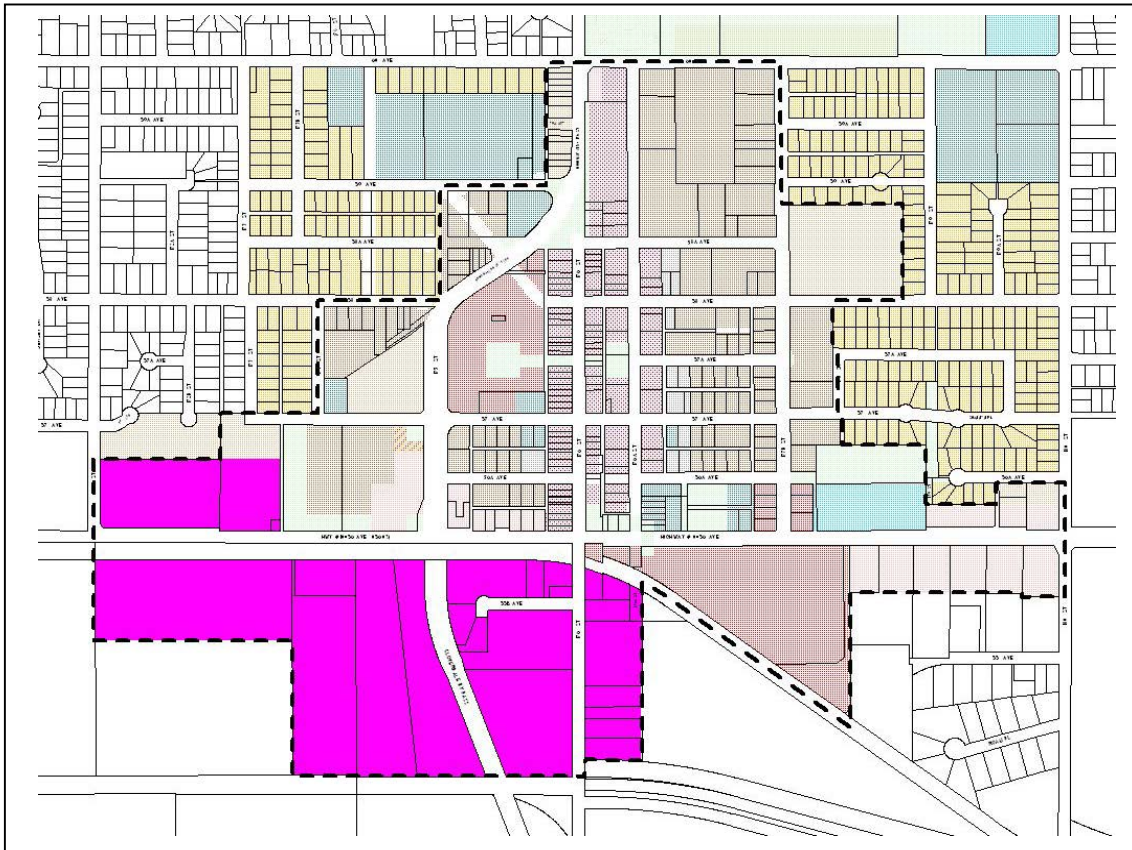


Figure No. 8 – Industrial Lands

Business growth from industrial developments in the surrounding areas will benefit the Town Centre's economy. An increase in employment through industrial growth can provide support to the Cloverdale market; but according to the Economic Study, the increased support would not be substantial. The Economic Study suggests that the Town Centre would receive greater benefits if the industrial lands were redeveloped for high density residential uses. While the Plan may view this as a possible long term option, it is not appropriate at this stage to suggest a change in land use from industrial to residential because there are more appropriate sites located closer to the Town Centre that are currently available for higher density residential uses. Also, the re-designation of industrial lands to other land uses would directly contravene the City's current policies and thrust for creating more employment centres. Therefore, the proposed Land Use Plan is recommending no changes to the existing industrial area south of Highway No. 10. The locations of these industrial areas are highlighted on Figure No. 8.

There is an existing industrial property (the Highway's works yard) located at the north west corner of Highway No. 10 and Highway No. 15 that could be considered for an alternative land use. Because of the site's close proximity to

the Town Centre and the expressed public desire to have a more attractive land use at this corner, one option is to allow for the development of a quality designed business park. This would be consistent with the current Industrial designation in the Official Community Plan and consistent with City policies pertaining to the provision of employment centres. Another option, as recommended by the Economic Study, would be to accommodate residential uses on this site because a residential population would provide more beneficial long-term support to the Town Centre than would a working population.

The easterly portion of the Highway's site (at the intersection of the two highways) is exposed to a high volume of truck traffic and noise from Highway No. 10 and No. 15. In view of this, it may not be appropriate for any form of multi-family residential. However, the southeast corner of the site is considered to be a prominent corner with the potential to accommodate an attractive landmark or gateway into the Cloverdale Town Centre. To achieve this, the easterly portion of the property could be considered for commercial uses and could be developed in a comprehensive manner in conjunction with either the adjacent future residential uses or with a business park component to the west. Possible commercial uses could include a hotel/motel and tourist-related services such as family restaurants and/or fast-food outlets. The Land Use Plan (Map No. 2) incorporates both land uses scenarios as sub-options to provide the flexibility.

H. Circulation

1. Vehicular Circulation

The Plan's proposed road pattern provides for more efficient circulation and improved accessibility to the Town Centre from the highways and from the surrounding residential community. An overview of the main roads and accesses to the Town Centre are schematically presented on Map No. 4. For north and south bound traffic, the primary access points to the Town Centre are from 176 Street/Highway No. 10 (south entrance), 176A Street/60 Avenue (north entrance) and 177B Street. The main access points for east and west bound traffic is the intersection of 58A Avenue and Highway No. 15, and 56A, 57, and 58 Avenues.

Several changes to the vehicular circulation network are proposed to consolidate a pedestrian-friendly Town Centre environment and to provide better vehicular circulation and access. The objectives of the proposed changes are oriented to:

- a) improve accessibility to the commercial core from the north, east and west residential areas,
- b) reinforce and support the proposed Town Centre core land uses,

- c) complete an interconnected pedestrian and vehicular circulation system, and
- d) reinforce a pedestrian-friendly streetscape with parking lots and a lane system at the back of the buildings.

The proposed changes to the existing circulation/street network in the Town Centre include:

a) *A New Direct Entrance to the Town Centre*

This new entrance from the north and west on Highway No. 15, at approximately 58 Avenue will provide direct access to the core commercial area and to the site of the Cloverdale Mall from the north and from the west. This new access will replace the existing one (see below) and will also provide adequate spacing between intersections along the Highway and a safe pedestrian crossing from the west residential areas. The implementation of this new entrance will occur in conjunction with the redevelopment of the Cloverdale Mall.

b) *Closure of the Existing North 176 Street Entrance to the Town Centre*

When the new entrance (described above) is fully operational, the existing entrance is proposed to be closed. This will eliminate awkward north access to the Town Centre for traffic from the Pacific Highway (Highway No. 15). Due to the road geometry of the Pacific Highway at this junction, the limited sight lines, and the awkward north-bound turning movements, the existing north entrance does not provide an obvious access point to the Town Centre. Also, the proposed closure of the north entrance at 176 Street will provide the opportunity to use the closed road rights-of-way to create an open space window and clear view of the Cloverdale Town Centre from traffic on Highway No. 15.

c) *Closure of 176A Street at Highway No. 10*

The existing intersection of 176A Street and Highway No. 10 is located relatively close to the fully signalized intersection at 176 Street and Highway No. 10. The 176 Street intersection has recently been improved and upgraded by the Ministry of Transportation and Highways and is the main south entrance into the Cloverdale Town Centre. The proposed closure of the 176A intersection will, in effect, improve safety and traffic flow on Highway No. 10 and will provide an opportunity for the City to develop an open space window and Town Centre

gateway at this location, thus improving the visibility of the Town Centre from traffic on the Highway. To maintain accessibility for those stores north of Highway No. 10 on 176A Street, 56A Avenue will be opened between 176A and 176 Street and will provide immediate access to the stores.

d) *Pedestrian Bridge Over Highway No. 10*

The proposed pedestrian bridge over Highway No. 10 (provided with elevators) will allow for the safe crossing of this regional route and link the Clover Square Mall and Red Barn node to the Town Centre core. The pedestrian overpass is expected to become a gateway feature that announces the Cloverdale Town Centre core to travellers on Highway No. 10. The structure may also be the focus of a public art initiative and will require substantial funds to construct for which sources must be investigated.

e) *Opening of 56A Avenue from 176 to 176A Streets*

This section of the right-of-way is currently closed and is used only as a pedestrian connection between the two main streets (176A and 176 Streets). The existing pedestrian walkway appears uninviting because the landscaping and sidewalks along the right-of way need to be upgraded. A small area of the right-of-way is currently being used for off-street parking. The proposed closure of 176A Street at Highway No. 10 will necessitate that 56A Avenue be opened between 176 and 176A Streets in order to facilitate traffic movement within the Town Centre area.

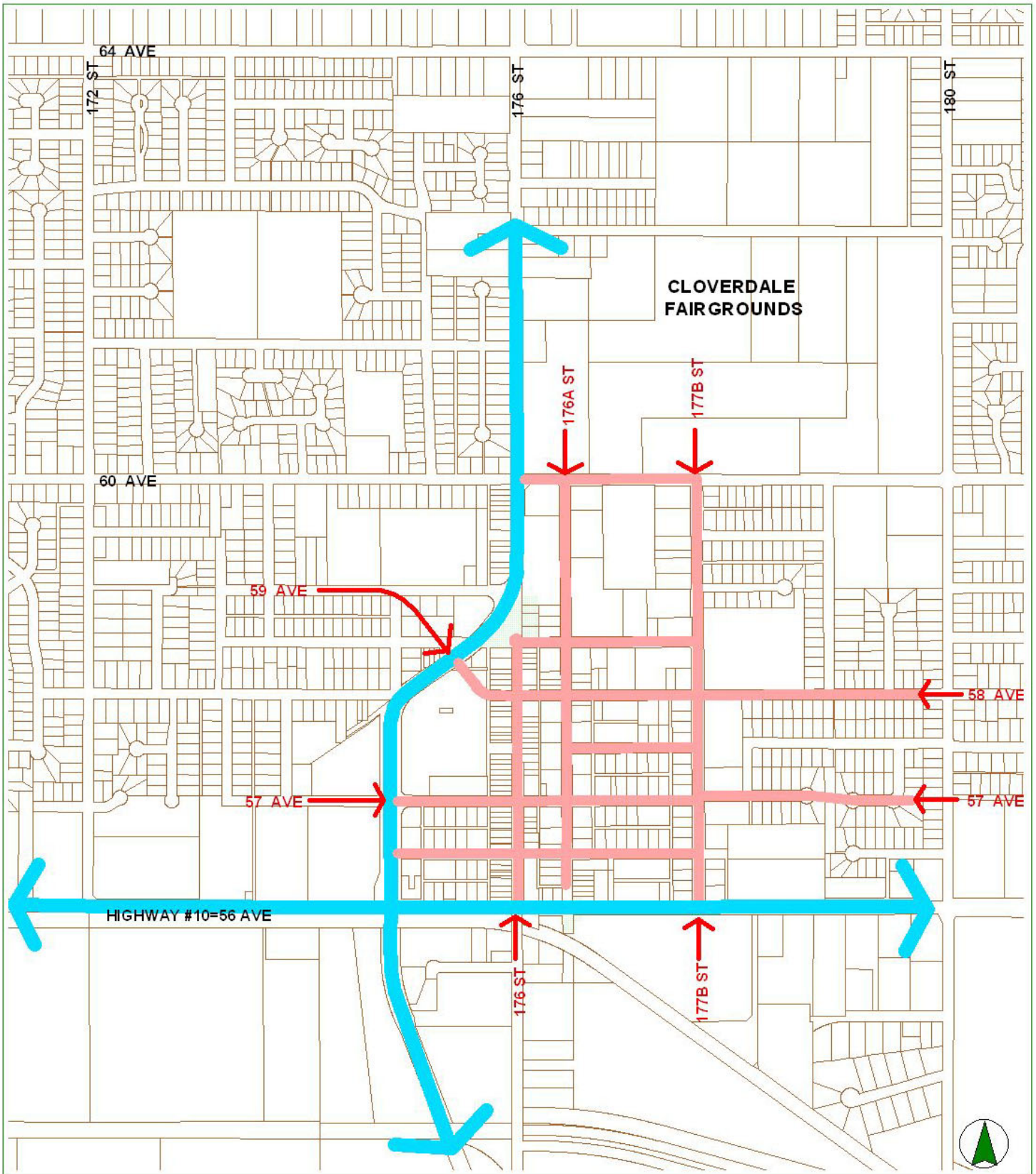
f) *City Public Parking Lots*

Small public parking areas which are accessible from the lateral streets and back lanes are proposed at the transition between residential areas and the core commercial area. Most of these parking lots are located on the east side, behind the 176A Street commercial area.

Parking with access from the rear lane is proposed for all commercial areas along the two main commercial streets. The treatment of the edges of these parking areas is intended to achieve street character continuity and to provide a soft transition between different land uses. Colonnades, trellises or similar structures should be used along the street and lane edges of the public parking lots to bridge the gap between commercial and residential buildings.

g) Minor Changes to the Street Pattern

Minor changes are proposed to roads within some residential neighbourhoods in order to provide more direct access routes to the Town Centre. These changes, involving road dedications and closures, will occur upon redevelopment of the area for higher density uses.



2. Pedestrian Circulation

An attractive and efficient pedestrian circulation system around and leading to the Town Centre area is crucial in order to encourage people to shop in the Town Centre rather than leave the area by car to conduct business elsewhere. The Plan proposes pedestrian-friendly corridors linking commercial activities, schools, parks, and open spaces with residential neighbourhoods, recreation facilities, and employment centres. An overview of the pedestrian linkages and circulation in the Town Centre is shown on Map No. 5.

It is important to provide easy direct access to the Town Centre from the residential neighbourhoods located on the west and east sides of the Town Centre. The Plan identifies 57 and 58 Avenues as the main east-west access routes linking the Town Centre with the residential community. These streets should incorporate wider sidewalks and bicycle lanes to promote walking and cycling opportunities. The streets should be lined with generous landscaping elements including trees to provide a pleasant walking environment. The existing road pattern (i.e., 57 Avenue east of 177B Street) will need to be modified through redevelopment opportunities to create these more desirable direct routes.

Another key linkage is the development of a strong pedestrian spine along 176A Street connecting the Town Centre with the Cloverdale Fairgrounds to the north and with the Clover Square Village Centre to the south. The Cloverdale Fairgrounds support a multitude of public activities and community facilities, and receive a large number of visitors throughout the year. The Plan recognizes the potential market that the Town Centre could capture from the Fairgrounds, and proposes easy and convenient access routes leading to the Town Centre to entice Fairground visitors to the Town Centre.

Another major market located within walking distance to the Town Centre is the Clover Square Village shopping centre on the south side of Highway No. 10 (directly south of the Cloverdale Town Centre). One of the issues raised by residents in the area is that shoppers at Clover Square Village are unlikely to walk across Highway No. 10 to continue their shopping trip in the Town Centre because of safety concerns associated with crossing the highway. The Land Use Plan recognizes the importance of integrating this shopping centre into the Town Centre concept by proposing a pedestrian highway over-pass at the south end of 176A Street. This pedestrian bridge would serve two purposes: to provide safe and direct access to the Town Centre from the shopping centre site, and to provide a significant landmark/gateway feature to announce one's arrival into the Town Centre from Highway No. 10. The bridge could add uniqueness to

Cloverdale through a special design and perhaps could be undertaken as a public art project.

Another significant pedestrian corridor will be located along the alignment of 57A Avenue from 176A Street west to the Cloverdale Mall site. The purpose of this corridor is to provide an east-west connection from the Town Centre to the new public multi-use area within the Cloverdale Mall site. This walkway could incorporate an agricultural theme, for example by accommodating the display and sale of farm products along the pathway leading up to the public gathering place/shopping centre.

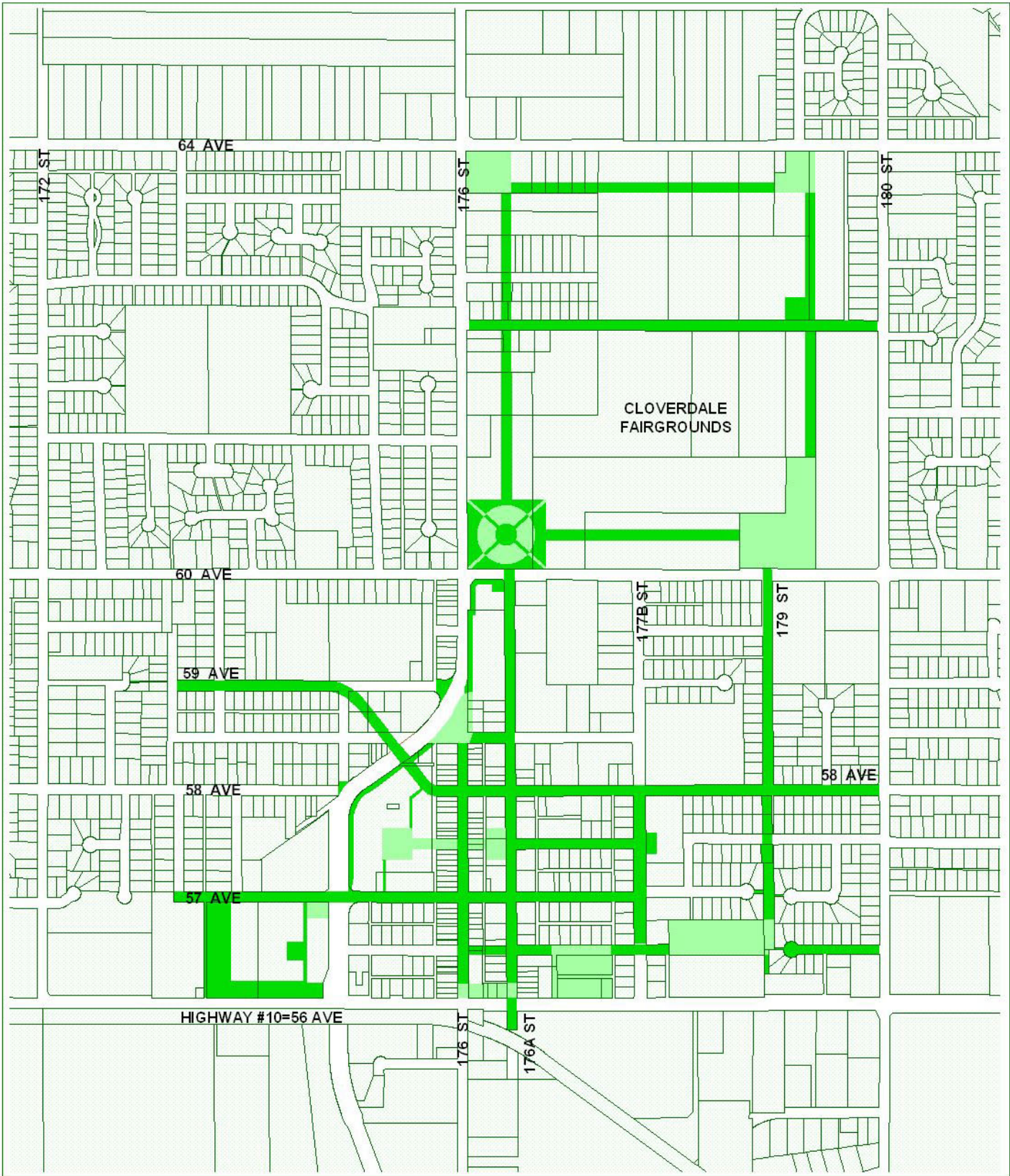
3. The Interurban Rail

The Interurban Revitalization Committee has undertaken an initiative to revitalize the use of interurban cars and track for operations in Surrey. The project involves the acquisition and restoration of three original electric interurban rail cars which would be used to inter-connect heritage/tourism sites along an existing 13 1/2-mile rail line between Cloverdale and Brownsville in South Westminster. The section of rail line in Cloverdale is located along the south side of Highway No. 10. At 176 Street, it will run southeast along the south side of the Clover Square Village Shopping Centre site. The implementation of this program will include the construction of a rail station on the Clover Square Village site close to 176A Street, which is the approximate location of the original inter-urban station.

The interurban rail project would provide an interesting alternative transportation mode to bring visitors to the Cloverdale Town Centre.

4. Heritage Streetcar Opportunity

With the revitalization of the interurban rail and a rail station stop at the south end of the Town Centre at 176A Street, there is an opportunity to reinforce the rail theme of the Town Centre core by the use of a heritage streetcar along 176A Street. A small Oporto 35-seat heritage streetcar would serve this purpose and it could be obtained and be activated with a track running along 176A Street from the proposed civic square at the south end of the Town Centre at Highway No. 10 to the Cloverdale Fairgrounds site up to the Millennium Amphitheatre at 64 Avenue. The streetcar could be viewed as a centrepiece of Cloverdale's revitalization and could provide visitors and local residents with a unique transportation experience.



PARKS/OPEN SPACE
 PEDESTRIAN CORRIDORS/BICYCLE ROUTES

Land Use Statistics

Table No. 3 below summarizes the projected development yield of the various land use types as designated by the Land Use Plan.

Table No. 3 – Land Use Statistics

| Land Use | Area (in acres) | FAR or Density ¹ | Floor Space (sq. ft.) or No. of Units | Persons Per Unit | Population ² |
|-------------------------------|--------------------|--------------------------------|--|------------------------|-------------------------|
| Town Centre Commercial | 16 | 1.5 | 710,000 sq. ft. 348 dwelling units | 1.5 | 522 |
| Retail/Service Commercial | 17 | 0.6 | 446,000 | - | - |
| Highway Commercial | 14 | 0.35 | 209,000 | - | - |
| Medium Density Residential | 46 | 30-45 upa | 1,380-2,700 units | 2.0 | 2,760-4,140 |
| Townhouses | 15 | 15-30 upa | 225-450 units | 2.5 | 563-1,125 |
| Industrial | 66 | - | Existing | - | - |
| Institutional | 5 | - | Existing | - | - |
| Parking | 2 | - | - | - | - |
| Parks & Open Space | 10 | - | - | - | - |
| Schools | 4 | - | - | - | - |
| TOTAL | 195 acres | - | 1,365,000 sq. ft. of commercial space 1,953 - 2,868 dwelling units | - | 3,845 - 5,787 |

¹ These densities are averages and do not necessarily represent the maximum allowable in the applicable zones.

² The population projections are for the long term because new dwelling units will only be created through redevelopment.

J. Cloverdale Town Centre Land Use Policies

One of the main objectives of the Plan is to formulate development strategies and policies that will revitalize the Cloverdale Town Centre and maintain it as a healthy and viable Centre. Achieving this objective will require implementation of the following policies.

1. Overall Development Policies

- a) The Cloverdale Town Centre shall be developed as the principal urban commercial centre in Cloverdale and shall provide a wide range of commercial uses and services including retail, office, personal service, entertainment, recreation, cultural, social and community facilities.
- b) The Cloverdale Town Centre's health and vitality should be protected by discouraging the development of major shopping and neighbourhood centres outside of the Town Centre.
- c) The Cloverdale Town Centre shall be promoted and characterized as a small town, family-oriented Centre with a rich history and a heritage-related atmosphere.

2. Economic Policies

- a) The Cloverdale Town Centre will be promoted and reinforced as a place for shopping and doing business by focusing commercial growth within the Town Centre core and discouraging it from spreading outward towards the residential areas or along the highways.
- b) Retail stores that provide a wide variety of goods and services in the Town Centre will be encouraged.
- c) Industrial and business parks should be developed within a short driving distance of the Town Centre to increase employment and business growth in the area.
- d) The industrial area south of Highway No. 10 should be maintained for light industrial and business park uses in order to create employment opportunities for the local residents and to ensure the existence of a working population to help support the Town Centre.

- e) Major recreation centres and community facilities should be located close to the Town Centre.
- f) Special events, festivals, theme attractions and special areas such as the Cloverdale Fairgrounds and Mound Farm (an urban forest park located to the west of the Town Centre south of Highway No. 10) should be promoted as year-round tourist attractions.

3. Commercial Policies

- a) To create a strong and vibrant Town Centre, service and highway commercial growth should be contained along Highway No. 10 within the existing highway commercial land use designation.
- b) Commercial activities that are oriented and designed to create a “people” place and high levels of pedestrian traffic should be encouraged.
- c) Street-oriented commercial and other pedestrian-friendly land uses (i.e., shopping, services) should be encouraged to locate in the Town Centre.
- d) To promote an interesting mix of activities, people and services, a diversity of land uses should be encouraged in the Town Centre including a variety of retail uses, sidewalk cafes, tourist-related uses and mixed commercial/residential developments.

4. Residential Policies

- a) To lend support to Town Centre’s growth and vitality, the concentration of residential uses in and around the Town Centre core should be increased.
- b) Senior’s housing should be encouraged and developed in close proximity to social amenities and Town Centre activities.
- c) Existing single-family areas located within walking distance of the Town Centre should be encouraged to be redeveloped for higher density uses in order to increase the area’s population.

- d) Appropriate design and interfacing should be considered for multiple family projects located adjacent to housing projects of different densities and built forms.

5. Circulation Policies

- a) Good direct access and linkages should be provided to the Town Centre from the residential neighbourhoods.
- b) The pedestrian routes to the Town Centre should be maintained and enhanced as attractive and well-lit streets with trees and landscaping elements.
- c) A strong link should be developed from the Town Centre to the area's community services, parks and recreational facilities.
- d) Public parking facilities should be provided close to the Town Centre to allow for convenient shopping, services and entertainment.
- e) Safe direct vehicular access to the Town Centre should be provided through the use of effective signage and adequate turning and queuing lanes off of Highway No. 10 and No. 15.
- f) Measures should be pursued to lessen traffic speeds along the highways where the highways cross through the Town Centre.
- g) Safe direct pedestrian access to commercial activities from residential neighbourhoods should be provided through the use of overpasses across the highways if necessary.
- h) Lanes servicing the commercial streets should be retained and the existing grid system should be reinforced to facilitate car access to rear parking lots and for services such as loading and garbage pick-up. The front streets would therefore be more pedestrian- friendly and ensure that an unbroken facade of commercial activity is developed. Lanes and/or frontage roads should be required for developments abutting arterial roads and highways.

6. Community Amenity Policies

- a) The Town Centre should be promoted as being an attractive place for living, shopping and doing business and therefore should contain a good mix of civic amenities and facilities either within the Town Centre area or within easy walking distance.
- b) A special ambience should be created in the Town Centre by promoting and developing quality urban streetscapes, mini-parks and public plazas to provide an interesting and active street life.
- c) Activities and programs in the Town Centre should coincide with events at the Cloverdale Fairground to achieve mutual economic benefits.
- d) A civic garden and activity area should be developed adjacent to the Senior's Centre and Library in order to provide visual interest and amenity space for seniors and the general public.

7. Heritage Policies ³

To emphasize Cloverdale's identity, the dominant role that Cloverdale played in the past as a railway junction and agricultural centre and its resulting town-site layout and architectural character will be expressed in many components of the Town Centre. The major means to achieve this character objective shall include:

- Strong local heritage-based architectural language;
- An integrated pedestrian network along with public spaces and streetscapes to emphasis Cloverdale's roots; and
- The use of public art to reflect historical events, places and celebrations.

The following policies should be used to guide the development of a heritage character in the Town Centre:

³ The heritage aspects of the Town Centre are further reinforced by the Urban Design Guidelines discussed in Section VIII. Section VIII also contains a detailed heritage inventory and history of Cloverdale which is used to guide the emerging character of the Cloverdale Town Centre.

- a) The Cloverdale Town Centre shall be developed and established as a unique Centre that is characterized by its pioneer spirit and heritage values.
- b) To enhance and reinforce the Town Centre's heritage theme, initiatives should be taken to retain and enhance the neighbourhood's older historically valuable buildings, develop design guidelines focused on the use of traditional design elements, and relocate well-maintained older homes of heritage value to the Town Centre.
- c) To create Cloverdale's special ambience, the development of specialty stores and businesses such as galleries, gift shops, and antique stores that would contribute to the enhancement of the Centre's unique atmosphere should be encouraged.
- d) The use of a heritage streetcar should be further investigated with a view to having it operate as a tourist and local attraction.
- e) To reinforce Cloverdale's historical context, the use of public art should be promoted and used to emphasize past events in reflecting the history and diversity of the area.

8. Urban Design Policies

The urban design policies are presented in detail in the following Section VIII - The Urban Design Concept Plan.

VIII. TOWN CENTRE URBAN DESIGN CONCEPT

A. Historical Background

1. Introduction

Cloverdale is the City of Surrey's first Town Centre. It was officially established in 1879 near Five Corners which is the historic intersection of 60 Avenue and Old McClellan Road. The commercial core of Cloverdale evolved to what now constitutes the focus of this Cloverdale Town Centre study. It is unique in character, reflecting its historical roots as a village centre within a rich agricultural region known as Clover Valley. The crossing of three railway lines provided many opportunities for the community and had a strong impact on its physical layout.

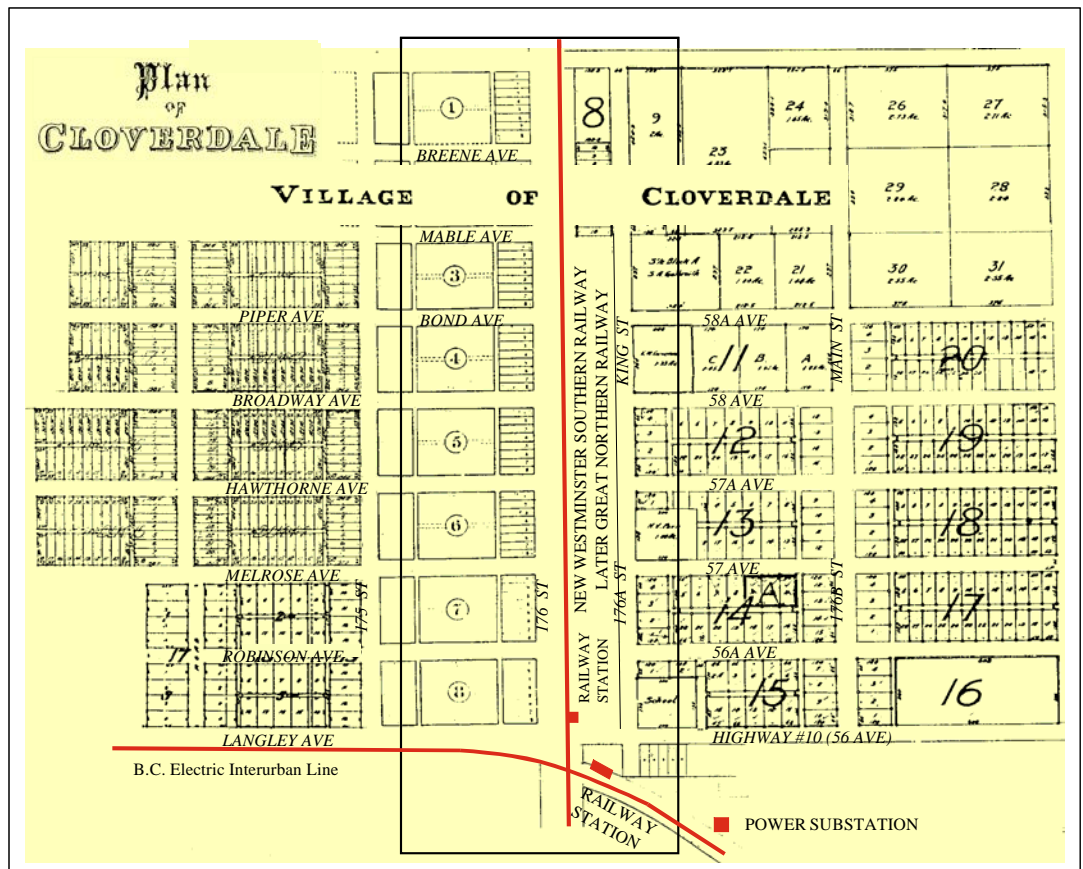


Figure No. 9 – Cloverdale's Original Subdivision Layout

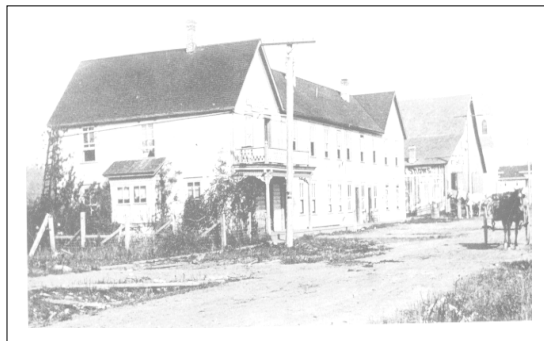
"The first European entrepreneurs and speculators came to Surrey in the mid-nineteenth century. The rich soil, abundance of fresh water supplied by artesian wells, temperate climate and considerable rainfall made the area, known as Cloverdale, easily recognizable as having great potential

for agricultural development. The area was first surveyed in 1859 and a small community formed around 1870”.

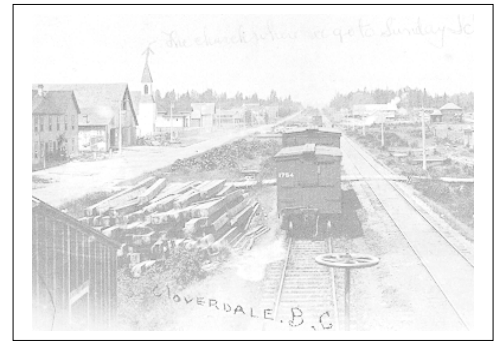
“It is said that a few days after arriving in the valley, in the year 1875, Mr. Williams Shannon had the occasion to write a letter and was in doubt as how to describe his home address. He looked out at the wild clover that grew abundantly everywhere and at once thought of Clover Valley. Soon afterwards he traveled to Victoria with a deputation to obtain a post office for the district and registered Clover Valley as the name.”

“Not long after the naming of Clover Valley, a road was constructed from the community to the Nickomekl River. It was at this junction that transport stern-wheelers brought basic provisions for the settlers and exported the farmer’s produce. The road was called the Clover Valley Road.”

“Joseph Shannon, who was involved in farming, became active in real estate with the prospect of the Southern Railway making a stop at Clover Valley. It was planned that the railway would pass through Clover Valley on its route from Bellingham to New Westminster. In anticipation, Joseph Shannon subdivided and sold some of his land to people speculating on the commercial development that would flourish around the train stop. In order to accommodate the men who would build the railway, John and Mary Starr built the Starr Hotel in proximity to the railway line (176 Street). They also purchased the adjoining property and opened a blacksmith shop and livery stable”.



Cloverdale Hotel and Livery Bams. West side of 176 St.



The Great Northern Railway. 176 St (1910)

“When the Southern Railway was actually built in 1891, Clover Valley was an obvious location for a station due to the many facilities already in place. The railway engineers named the station “Cloverdale”, which was soon adopted as the town name”.⁴

⁴ From: *Surrey’s Heritage. A Selection of Surrey’s Historically Significant Buildings.* Volume One. Produced by the Surrey Heritage Advisory Committee. 1993.

b) Railway-Related Historical Landmarks in the Cloverdale Area

In 1891, the New Westminster Southern Railway opened. This railway had a run of twenty-two miles long, and was the only Canadian railway to be built without government subsidy. Its line followed the Fraser River upstream then to the east to “Liverpool Station” and “Port Kells”, then south to “Clover Valley” (Cloverdale), to a flag stop in “Hazelmere” and then to the border. This railway was taken over by the Great Northern Railway soon after completion. The railway line, as it crossed Cloverdale, followed the present day alignment of 176 Street.

In 1910, the British Columbia Electric Railway (B.C.E.R.) commenced operations. This railway line extended from New Westminster to Chilliwack. Cloverdale had a station and a station agent which helps to illustrate the importance of Cloverdale in the region. The tracks are still in place and are in use for freight cargo.

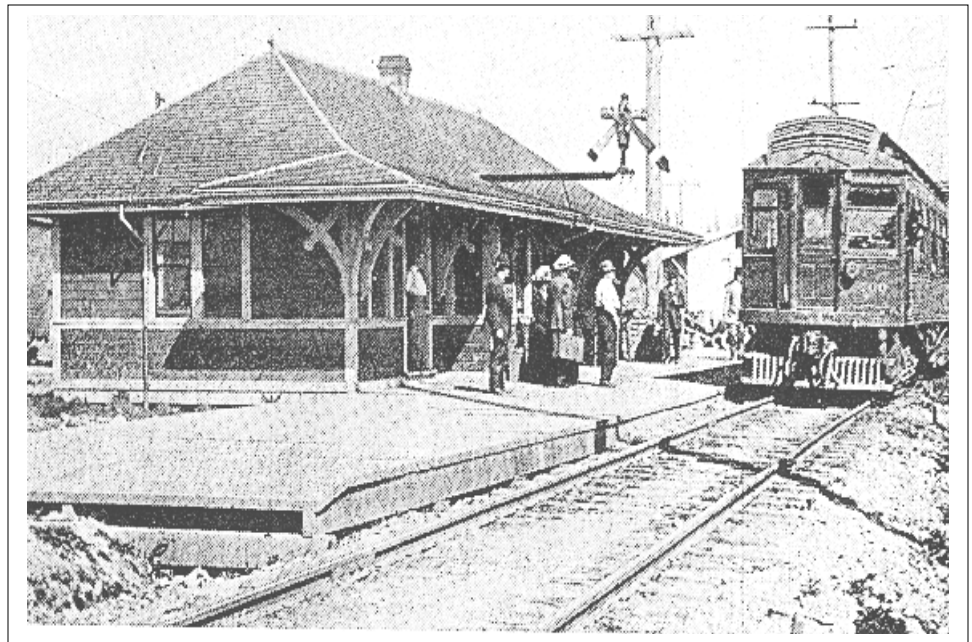


Figure No. 11 - The B.C.E.R. (British Columbia Electric Railway) at Cloverdale Station (1911)

The advent of the car reduced the importance of the railway in the area. The attention focused on road construction and the Pacific Highway (also following the alignment of 176 Street) was built. This new highway determined the decline of the role of Cloverdale as a transportation hub.

c) Agricultural Activities

Cloverdale was also the centre of some of the richest agricultural lands in the Fraser Valley. Its commercial area provided for the needs of families which settled in the area to clear and cultivate these lands.

The growth of the agricultural activities in and around Cloverdale and the Fraser Valley was also influenced by railway operations in the valley. The operation of the British Columbia Electric Railway (BCER) changed the economy of the Lower Fraser Valley by having a strong impact in the local dairy industry. It allowed the farmers to ship milk to the Vancouver market; prior to this reliable means of transportation, dairy farmers in parts of the Fraser Valley that were not close to the delta area had to ship their products to local creameries for processing into butter. By the summer of 1911 the milk business had increased so much that the BCER had to put on a special milk and vegetable train to carry the milk to Vancouver.

Cloverdale was also the location of the Surrey Co-operative Association. Established in 1921, the Co-op quickly became the dominant commercial enterprise of the district, providing farmers with a dependable outlet to sell their produce and purchase their supplies. The Co-op enterprise continually expanded for many years, adding a poultry killing plant, feed mills, a hardware and grocery store, a gas station and a lumberyard.

The agricultural/farming activity has continued in the area, taking advantage of the efficient transportation routes which link Cloverdale to regional markets.

3. Significant Heritage Buildings and Sites

There are several existing important heritage sites/buildings located within the Cloverdale Town Centre. Some of these have been identified and are contained within Surrey's "Heritage Register". The Heritage Register is a list of buildings, structures and other features with qualities or characteristics that are recognized as significant and contribute to the City's heritage. Several of these buildings have been officially designated as heritage buildings. Some are not "designated" for heritage protection, but would be subject to review before being demolished or substantially modified.

All of the sites on the Heritage Register are excellent examples of Cloverdale's architectural past. They are listed in Appendix A, along with a summary of their heritage values or dominant characteristics.

B. Design Objectives and Principles

The Plan intends to inject a new vitality, develop a strong sense of place and establish an identifiable character for the Town Centre by building on its unique railway and agricultural past and making use of the authentic heritage/historical roots of the town site.

To achieve the overall character objectives and reinforce the viability of the proposed land uses, a comprehensive vision for the development of the Town Centre area becomes necessary. Realization of this vision will require the implementation of coordinated site-specific recommendations and design guidelines aimed at reinforcing the unique local heritage.

The expression of the heritage component in the Town Centre area need not be restrictive but open to explore forms and modern building designs and materials that are able to bring mental images of the Town Centre's past to the present day urban spaces.

The overall heritage character of the Cloverdale Town Centre is not proposed to be achieved by copying and reproducing old building forms and details, but through a contemporary interpretation of these architectural forms and details. A good example of this principle is the strong image associated with the era of the Tall Ships generated by the Convention Centre building at Canada Place (e.g., contemporary architectural design which makes use of modern technology and materials).

C. Reinforcement of the Heritage Character of the Town Centre

The successful reinforcement and establishment of a unique and distinctive character for the Town Centre area of Cloverdale will require the coordinated action by the community, private sector and local government.

Implementation of a consistent and systematic use of heritage-related elements/images throughout the Town Center will help to guide new development and redevelopment.

The use of objects, images, history and forms (including architectural forms) associated with the railway and agricultural past of Cloverdale can be incorporated into the urban fabric or streetscape of the Town Centre to reinforce its character and to tell the story of the Cloverdale town site. Some of these include:



Small, simple form, wooden structures. White trim.



Large overhangs and wooden braces. Strong colours.

1. Using strong heritage architectural elements based on the railway and agricultural past of Cloverdale (including the Rodeo).
2. Consolidating the unique grid street layout found in the Town Centre and making use of the original street names.
3. Reinforcing an integrated network of pedestrian linkages, public spaces and streetscapes that emphasize Cloverdale's roots.
4. Using local pioneer family names or local heritage themes for streets, plazas, open spaces, buildings, etc. and identifying the original locations of the first businesses and places of residence of original well-known families.
5. Incorporating a public art component into the streetscape to reflect historic events, places and celebrations unique to the area.
6. Promoting sign and street furniture designs with heritage character for both businesses and streets.

D. Heritage Character and Public Art

Public art is also expected to play an important role in telling the story of Cloverdale. It should be located throughout the town site's public open spaces, building facades, street furniture, etc.

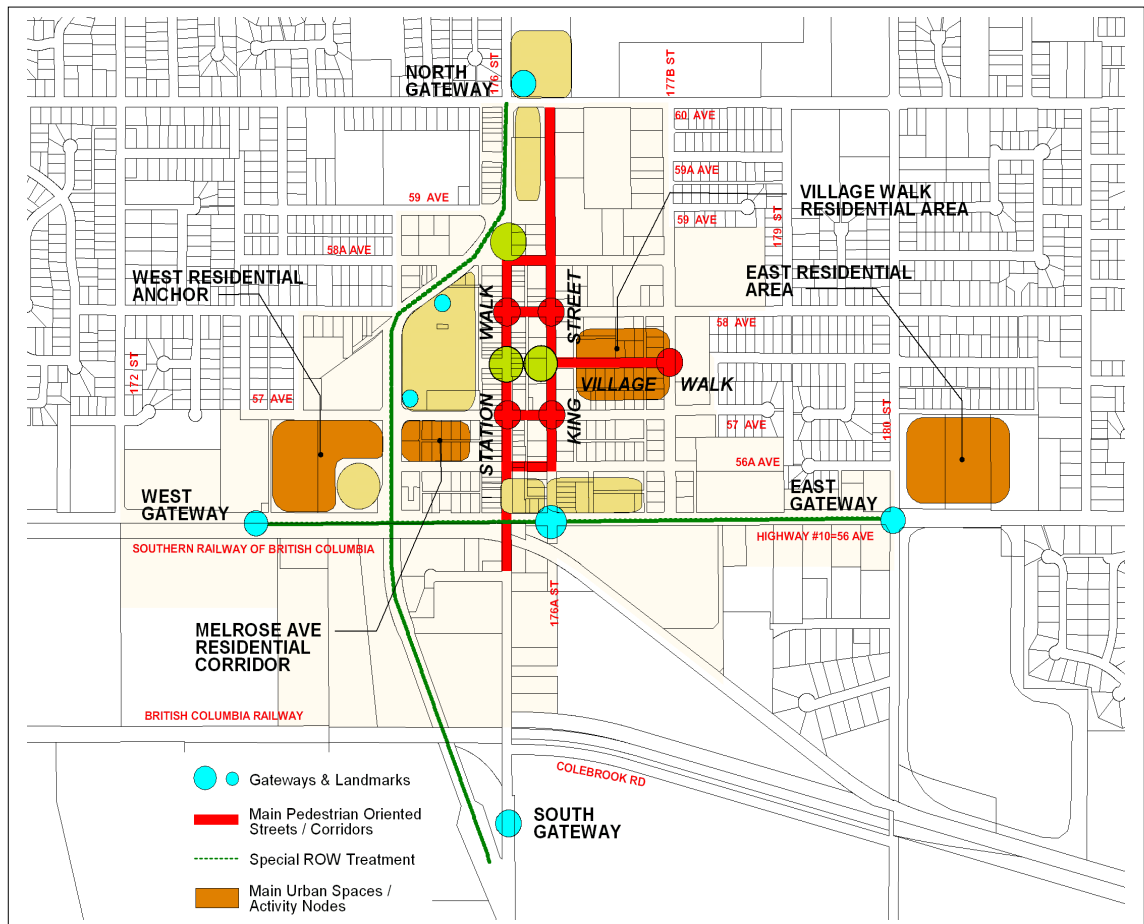
Public art elements are considered important components of the Town Centre urban streetscape that will provide the link between the past, present and the future. They are intended to reinforce the character, support the proposed land uses, restore a sense of belonging and ingest meaning to the Town Centre.

Sculptures, commemorative plates in metal or unique material inserted in the sidewalk pavement surface, fountains, benches, bollards, murals, building facades, objects and street elements associated with the railway and agricultural themes will play an important role in bringing the flavour of old times (and associated emotional attachments) back to the present.

In summary, a strong heritage character for the Cloverdale Town Centre will only be achieved through careful interpretation and step-by-step implementation of design recommendations regarding building design, streetscape and urban form, and the incorporation of public art as an integral part of the development process.

E. The Town Centre Urban Structure and Overview of the Urban Design Concept

The proposed urban structure for a lively and unique character atmosphere for the Town Centre is based on two **main pedestrian-oriented axes**, various public **urban spaces/activity nodes** along with **medium density residential areas** as anchors, and a series of **gateways/landmarks** along **perimeter routes**.



Map No. 6 - THE TOWN CENTRE URBAN STRUCTURE

The role of each one of these components is as follows:

1. Pedestrian-Oriented Streets and Corridors

These streets provide the basic grid of the Town Center core. The north-south axis is defined by “The Station Walk” (176 Street) and “King Street” (the original name of 176A Street). These two streets contain most of the commercial activities and have a strong railway related feel. The east-west axis is defined by “The Village Walk” (57A Avenue). It provides a strong direct link from the eastern residential areas (with a proposed higher density) to the commercial core.

2. Main Urban Spaces and Activity Nodes

Located within walking distance, these new and or redeveloped urban spaces and activity nodes are located along the main pedestrian streets. Some will also act as anchors or destinations along these pedestrian-oriented corridors. These spaces will act to build and reinforce the heritage storyline throughout the Town Centre core and provide reference points and landmarks for the visitor.

3. Residential Areas in or Near the Town Centre Core

The Town Centre residential areas are proposed in close proximity to the commercial core. These will allow for an increase in the number of residents in the area, provide for various types of housing accommodation and support the retail/commercial activities in the Town Centre.

4. Gateways/Landmarks and Perimeter Routes

These visual markers will identify the approaches and entrances to the Cloverdale Town Centre area. The characteristics of the perimeter routes around the Town Centre (i.e., Highway No. 10 and Highway No. 15) area will announce the different physical character of the Town Centre area to travelers as they drive by and through the Town Centre.

F. The Town Centre Urban Design Concept Plan

The Urban Design Concept provides direction for the comprehensive and coordinated physical development of the Town Centre while also reinforcing its unique, identifiable character.

In support of the recommended land use in the Town Centre area, the concept focuses on the improvement of the circulation/street network, and the establishment of the railway and agricultural-related past of Cloverdale as the common character theme for the core area.

1. The Dominant Character Theme

While providing a direction for the comprehensive and coordinated physical development of the Town Centre, the Urban Design Concept incorporates the railway and agricultural-related past of Cloverdale as a common theme that will be present in the streetscape and the architectural forms, as well as in the character and details of proposed developments (i.e., public urban spaces and activity nodes).

As a unifying element, steel rails imbedded in the sidewalk pavement or a simulated railway track imprinted in the sidewalk are proposed along “The Station Walk” (176 Street, the main north-south axis). These rails will remind us of the original alignment of the tracks of the New Westminster Southern Railway (later the Great Northern Railway).

Starting at the southern entrance into the Town Centre on Highway No. 10, this simulated railway line will link various public urban spaces/activity nodes along its way.

The tracks may serve as a guide for visitors to the Town Centre and will provide the opportunity for developing the railway storyline as it crosses the Town Centre core. The tracks end at the Town Centre’s northern anchor and gateway to the Fairgrounds on 60 Avenue. There is also potential for them to be extended northward to the site of the amphitheatre at the 64 Avenue and Highway No. 15 intersection.

To strongly consolidate the Town Centre theme, the potential to run a trolley/streetcar along “King Street” (existing ROW is adequate for this purpose) should be further investigated. Furthermore, implementation of the Urban Design Concept, in combination with the potential operation of a Town Centre streetcar and the revival of the “Interurban Rail” (with a station in Cloverdale), provide a tremendous opportunity for turning the Cloverdale Town Centre into a major regional tourist attraction.

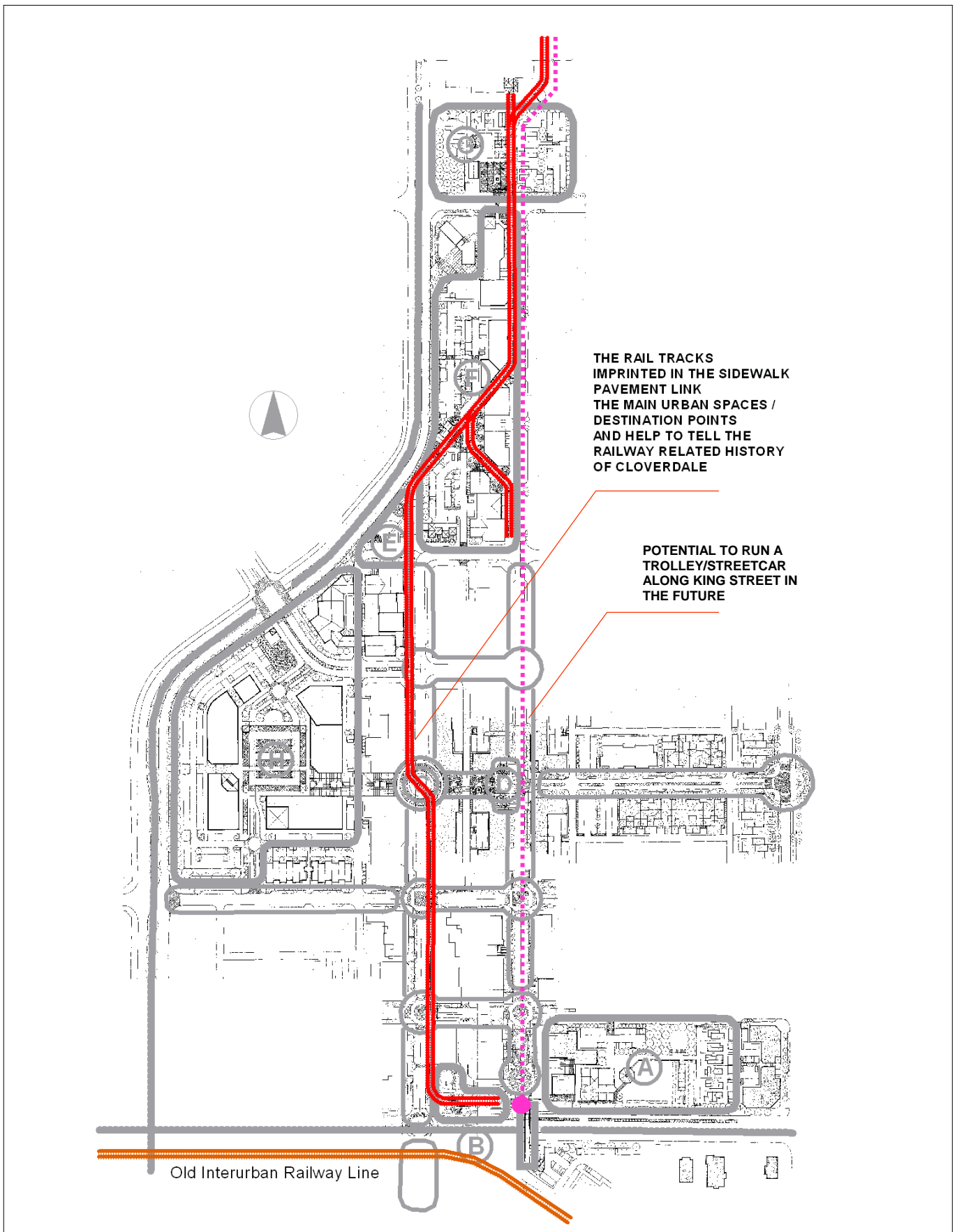
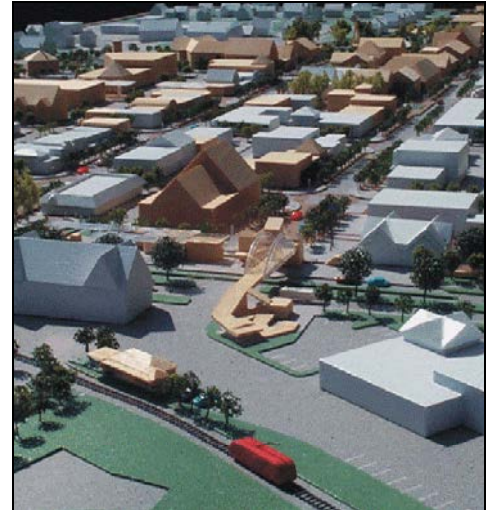


Figure 12. The simulated railway tracks are the unifying element linking major public urban spaces/activity nodes

Construction of a pedestrian overpass on Highway 10 to allow safe crossing to the existing Clover Square Mall on the south side of the highway would at the same time become the linkage between the two rail lines mentioned above, and would define a strong landmark to identify the Cloverdale Town Centre on Highway 10.

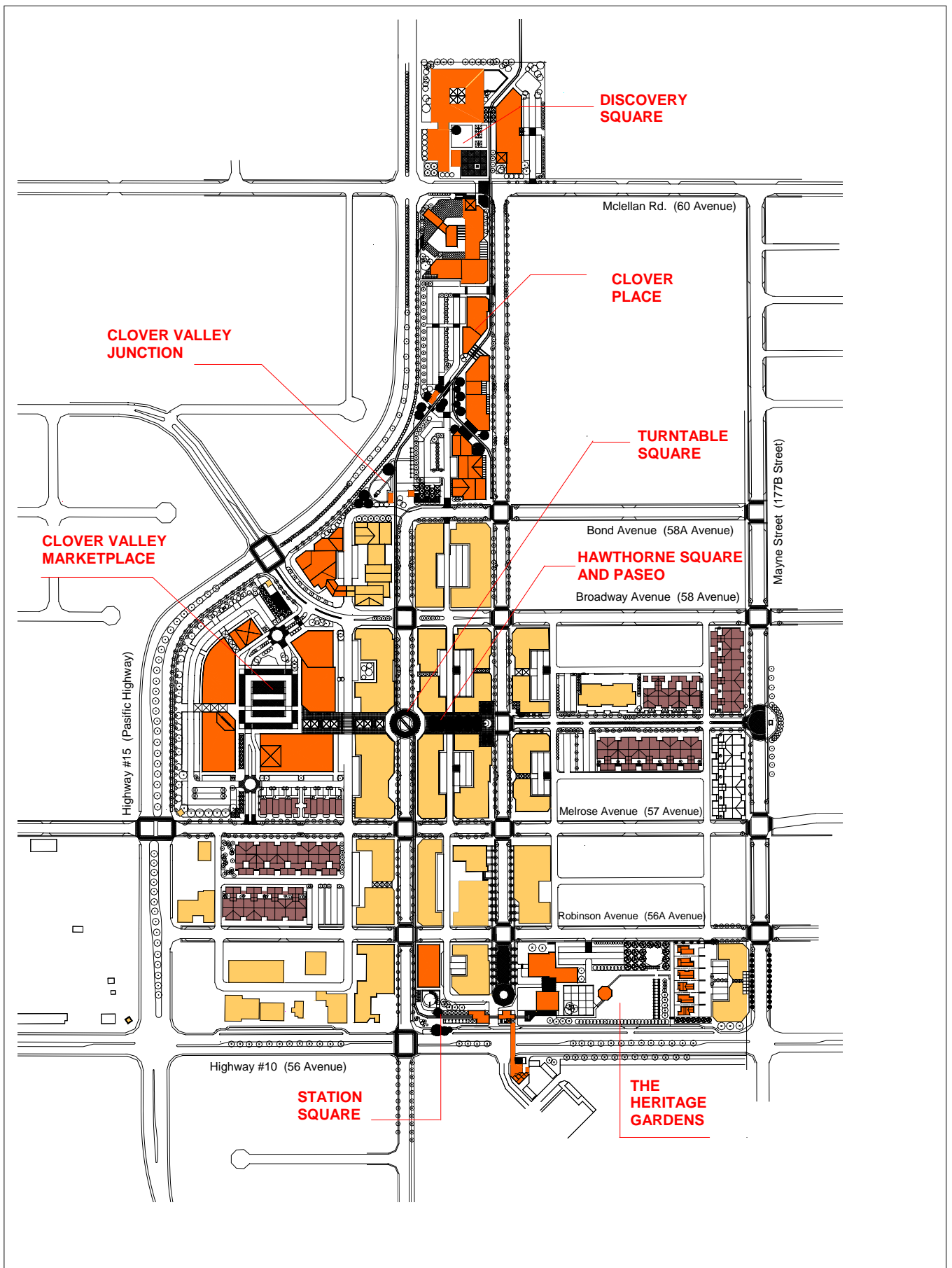


The revival of the Interurban Railway and the proposed pedestrian overpass on Highway No. 10 provide unique opportunities to incorporate character elements and features that will reinforce the railway theme developed throughout the Town Centre. The pedestrian overpass could eventually connect the Interurban to the streetcar along the Cloverdale Town Centre.

2. The Implementation Tools

To translate the overall character development objectives into a reality the Urban Design Concept Plan provides direction and articulates a series of general implementation tools. These are as follows:

- a) A comprehensive plan/vision for the physical development of the Town Center that achieves the overall design character and land use objectives.
- b) Identification of improvements to the streetscape required to achieve the ultimate vision.
- c) Formulation of design recommendations (terms of reference and general design guidelines) for development of the various site-specific urban spaces/activity nodes in the Town Centre.
- d) Formulation of a series of general building design guidelines applicable to any development (redevelopment) in the Town Centre area.



Map No.7. THE TOWN CENTRE URBAN DESIGN CONCEPT PLAN

G. The Main Components of the Urban Design Concept

1. Pedestrian-Oriented Streets and Corridors

The North-South Axis **“The Station Walk”** (176 Street) and **“King Street”** (176A Street)

This axis is established by parallel pedestrian-oriented commercial streets that will convey strong character images related to the railway. These parallel streets define the central spine of the Town Centre and link the southern window into the Town Centre with the gateway to the Cloverdale Fairgrounds to the north.

“The Station Walk” (176 Street) would concentrate most of the commercial activities in the area and “King Street” (the original name of 176A Street) would include a mix of commercial and office uses. “The Station Walk” follows the alignment of the old railway tracks along 176 Street, and extends from Highway No. 10 on the south (See Figure 14) to 58A Avenue, where it swings to the east and joins “King Street” to continue north to its intersection with 60 Avenue near the Pacific Highway

“King Street” extends in a straight line from the “Civic Street Node” which comprises the Library and the Senior’s Centre (Old Town Hall) near Highway No.10 (See Figure 15) to its intersection with 60 Avenue at the north end of the Town Centre. The continuity of building frontage and an already established street grid pattern provide the conditions for strong retail pedestrian activity at grade level along both of these main pedestrian corridors.

The East-West Axis: **“The Village Walk”**

This pedestrian-oriented corridor follows the alignment of 57A Avenue and extends from “Main Street” (177B Street) on the east to “King Street” (176A Street) on the west. From King Street (176A Avenue), it continues west as a pedestrian commercial street to “The Station Walk” (176 Street) and further to the redeveloped Cloverdale Mall site. This corridor provides a strong linkage between the eastern residential areas and the Town Centre commercial core (See Figure 16).

“The Village Walk” would convey images and a strong flavour associated with a village residential street. Street-oriented higher density residential development (stacked townhouses, row housing, etc.) with a strong single-family character is expected to line both sides of “The Village Walk”.

(For Design and Character recommendations see Appendixes A and C).

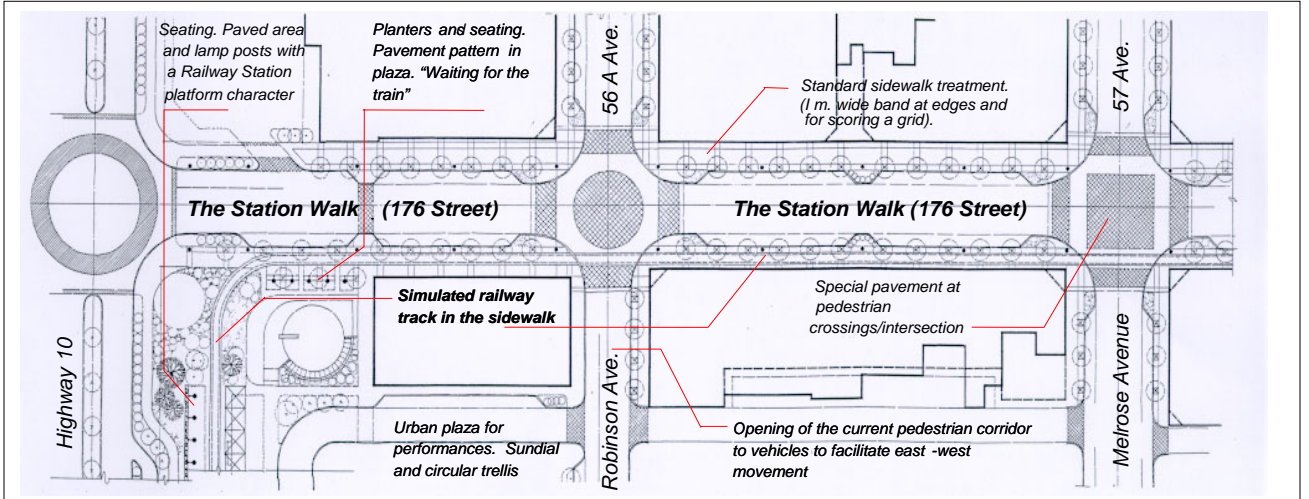


Figure No. 13 - Station Walk at the entrance to the Town Centre

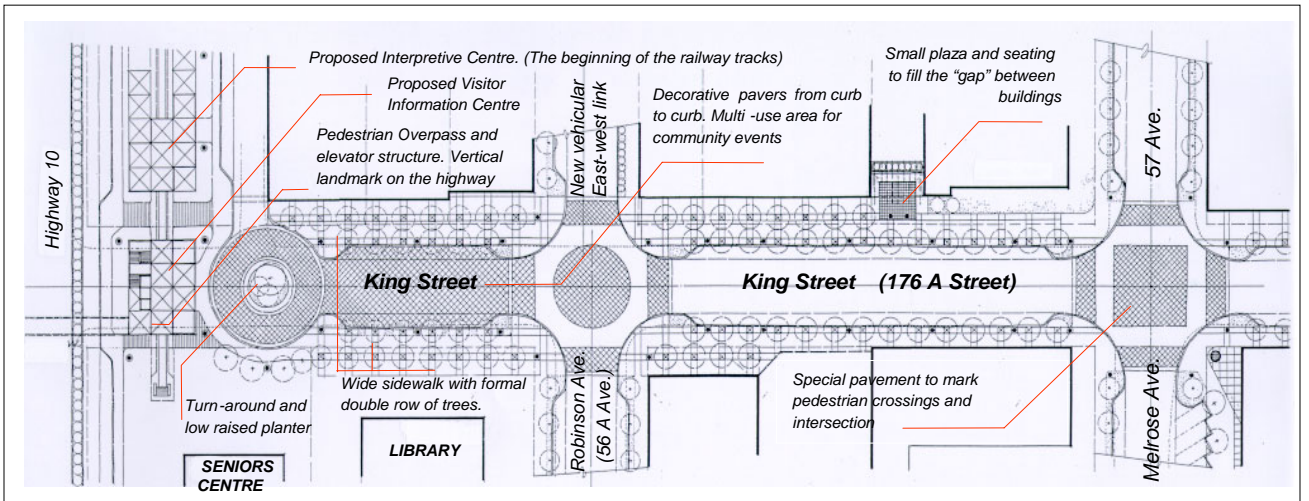


Figure No. 14 - King Street at intersection with Highway No.10 and turn-around

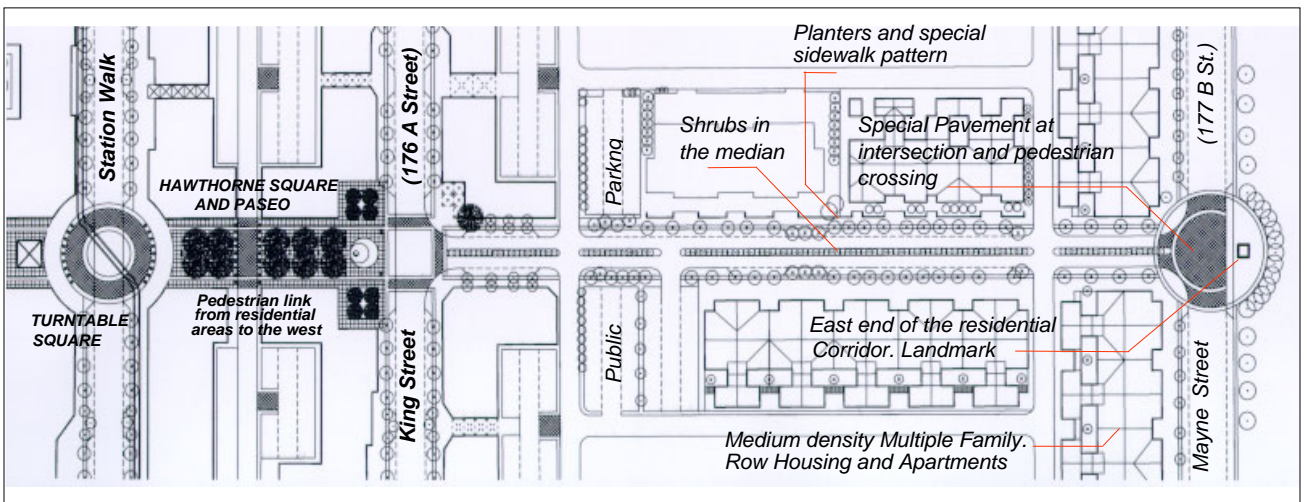


Figure No. 15 – The Village Walk Residential Corridor (57 A Avenue)

2. The Main Urban Spaces and Activity Nodes

These are special development sites in the Town Centre that because of their land use, strategic location and unique building character may become destination or reference points for residents and visitors to the Town Centre.

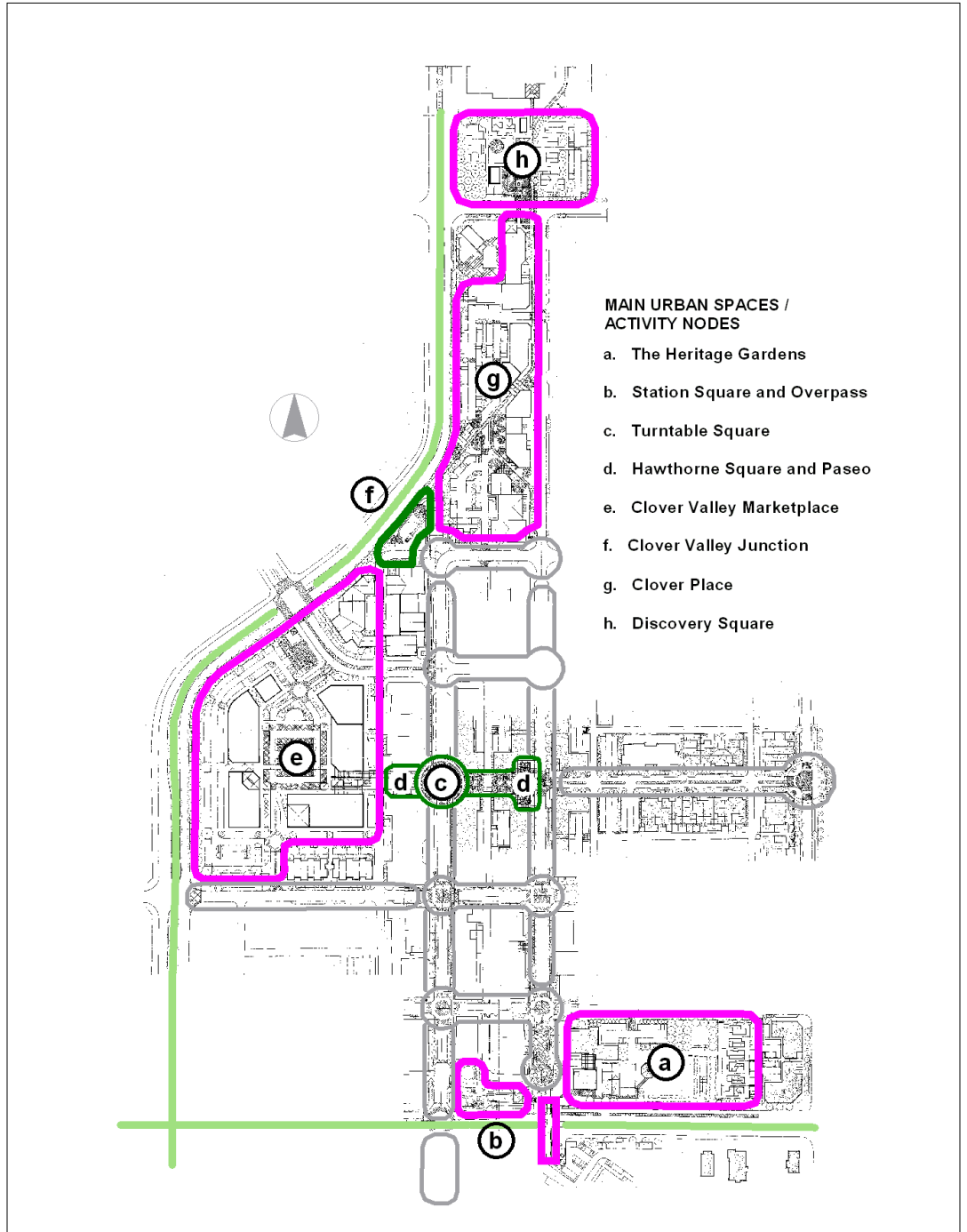


Figure No. 16 – Main Urban Spaces / Activity Nodes

The character and heritage details incorporated into the design of these activity nodes/urban spaces/developments will help to develop and communicate the heritage image and unique character of the area. These site-specific nodes are further described below.

a) The Heritage Gardens

This is the main open space on Highway No. 10. It signals the location of the Cloverdale Town Centre to westbound travelers approaching the area. It is an open space and garden that interrupts the continuity of highway commercial uses along the highway and exposes feature landscaped areas and local community activities to views from the passersby (See Map No. 8 for design and character recommendations). The Senior's Centre building (old Town Hall) sets the heritage character of the area.



The Heritage Gardens, the Seniors Centre and the overpass on Highway No. 10 open views and act as the welcoming vestibule into the Town Centre. Heritage homes define the eastern end of the Gardens.



Band stand and the trellis (roses) as dominant features within the open area of the gardens.

The north and east edges of this large open area may accommodate heritage houses of an architectural value that need to be relocated from their original sites. These structures would reinforce the character of this node while also providing housing opportunities for seniors.

b) Station Square

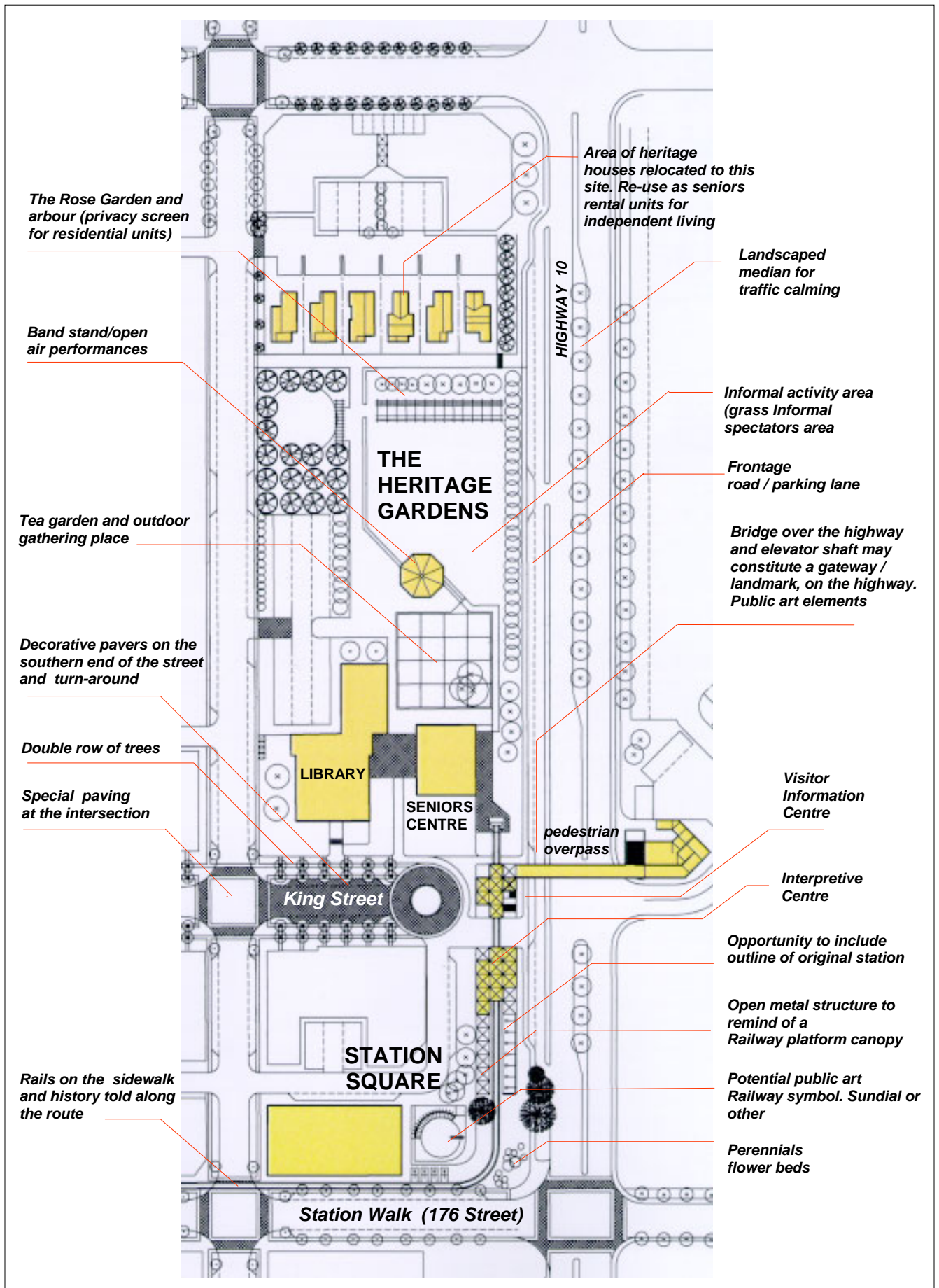
This urban space provides a window and defines the southern entrance to the Town Centre. The Station Square area, which is also supported by the “The Red Barn” on the south side of the highway, introduces the railway theme and related images as the dominant feature of the Cloverdale’s commercial core (See Map No. 8 for design and character recommendations).



Station Square (lower left) at the southern entrance to the town centre and the Overpass (strong landmark)

Opportunities exist at this location for the development of an Interpretive Centre with a strong railway station character - a place where the railway and agricultural past of Cloverdale are introduced to the visitors.

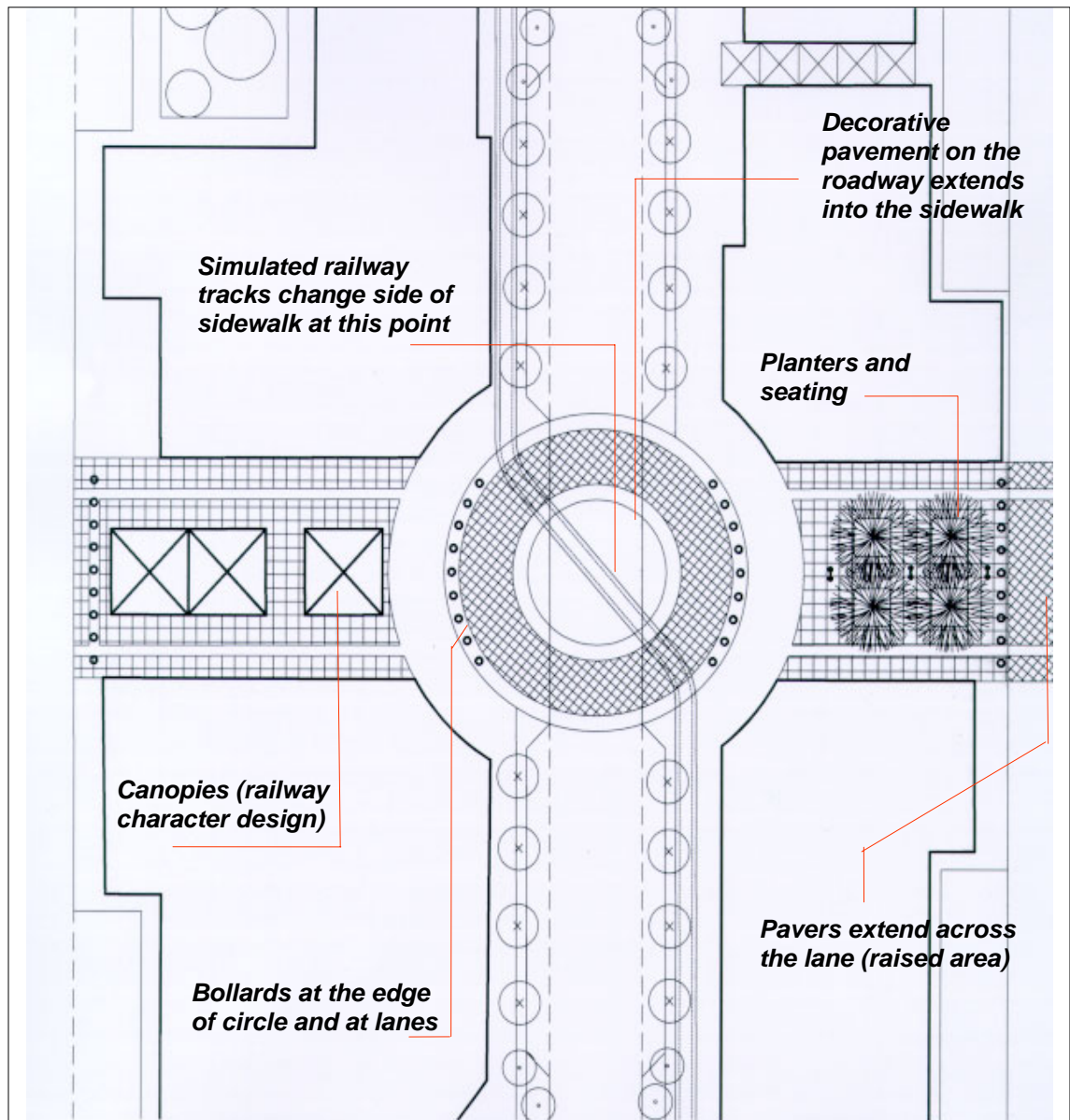
The simulated railway line that runs along the Town Centre core is proposed to start at this Centre, which is directly associated with the structure of the pedestrian overpass (containing an elevator and stairs). The overpass is a strong landmark that announces Cloverdale to travelers on the highway and provides the linkage between the Town Centre and the existing Clover Square Mall on the south side of Highway No. 10.



Map No. 8. HERITAGE GARDENS and STATION SQUARE

c) Turntable Square

This space identifies the intersection of the two main pedestrian-oriented spines in the Town Centre: the “Station Walk” (north-south); and the “Village Walk” (east-west). At this place, the simulated railway track on the sidewalk changes alignment from one side of the street to the other. The circular pavement feature reinforces the images of a turntable and its function in the railway operations. The design of buildings around this urban space is expected to reinforce these images and convey the feeling of a roundhouse. This urban open space is directly associated with “Hawthorne Square”, the gateway to the redeveloped Cloverdale Mall site to the west.



Map 9. TURNTABLE SQUARE. Identifies the crossing of the two Pedestrian-Oriented Streets

d) Hawthorne Square and Paseo

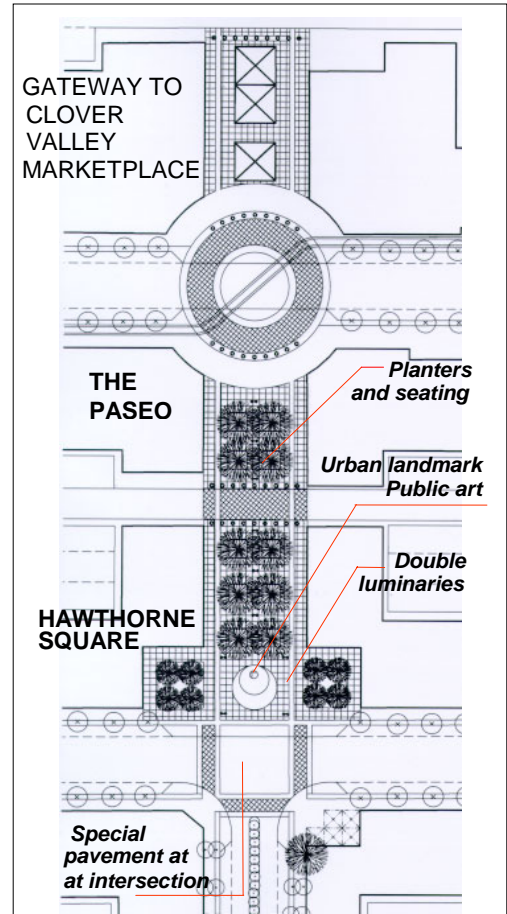
This open space is the gateway to the redeveloped Cloverdale Mall site and the western end of the “Paseo”; the commercial pedestrian street that is the extension of “The Village Walk” (57A Avenue) to the west. The formal character of this linear pedestrian commercial street known as the “Paseo”, offers opportunities for multiple uses which may range from a place to sit and rest, to an open-air display area and community activities.



Street Furniture. Planters/benches



“The Paseo”, “Turntable Square” and “Hawthorne Square” looking west toward “Clover Valley Marketplace” (the site of the present day Cloverdale Mall).



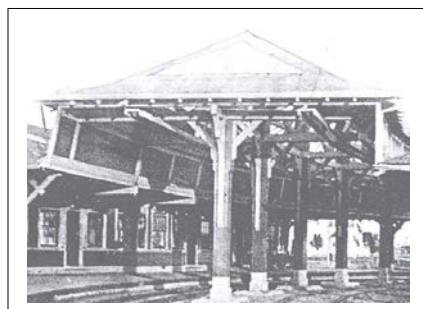
Map 10. HAWTHORNE SQUARE/PASEO

This pedestrian link between “The Station Walk” (176 Street) and “King Street” (176A Street) provides variety in the streetscape and breaks the excessive length of the central block at the core of the Town Centre.

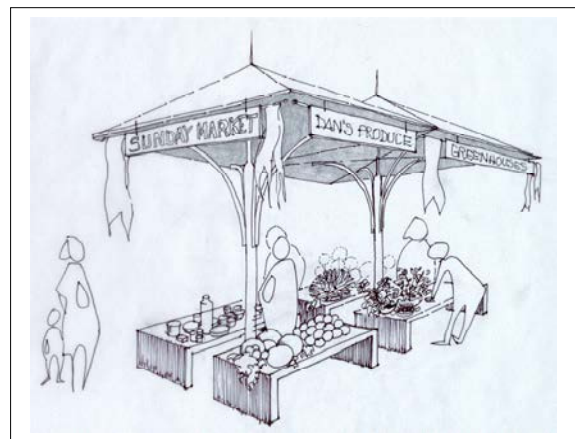
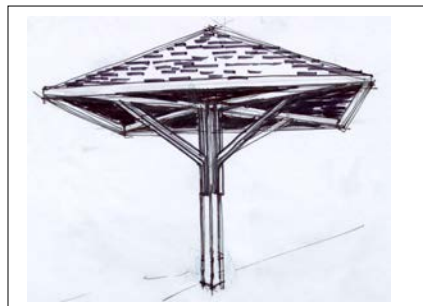
e) Clover Valley Marketplace

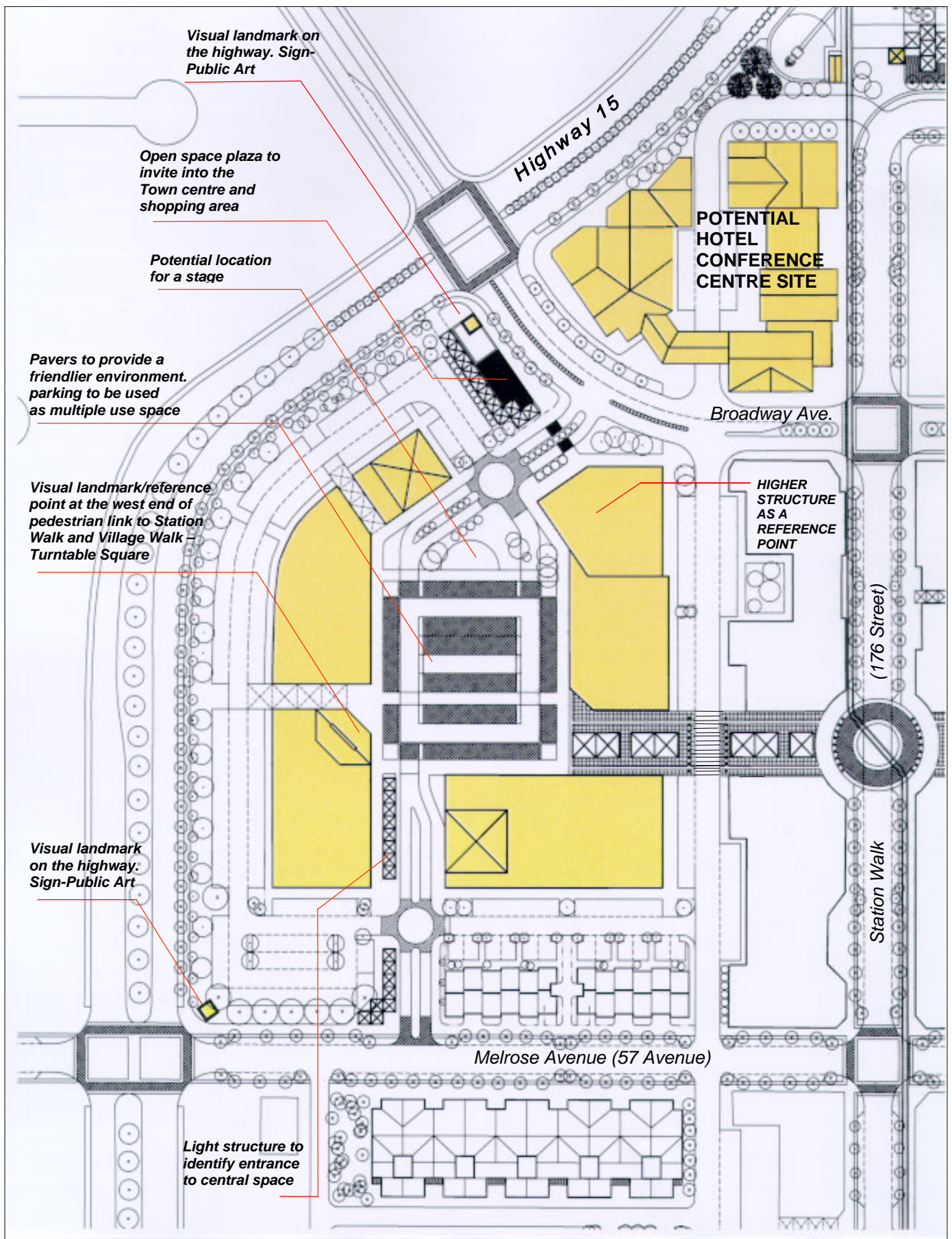
This commercial node will be the result of the redevelopment of the old Cloverdale Mall (See Map No. 11). The central parking area is proposed to be developed as a multi-use space. This central open space, preferably enclosed on three sides by buildings, will play the role of a small village marketplace or similar activity on Sundays and provide opportunities for large community gatherings and performances (i.e., Canada Day and Rodeo-related community activities).

Clover Valley Marketplace is the dominant commercial node in the town centre and the western anchor of the “Village Walk”. Located at the new highway entrance from the west, it also serves the surrounding areas.



Views toward the Clover Valley Marketplace from the northwest, across Highway 15. Notice the free standing identification sign and plaza as urban landmarks





Map No. 11 – CLOVER VALLEY MARKETPLACE

Known as the Clover Valley Marketplace, this commercial development will be the western anchor of the “Village Walk” (east-west axis). Pavement texture and pattern should be used in the multi-use area of the parking lot to communicate the intended uses and establish the dominance of pedestrians over vehicles.

f) Clover Valley Junction

This open space, at the northern end of the “Station Walk” (176 Street) provides a window into the Town Centre to southbound travelers on Highway No. 15. It introduces the transition from a railway theme to an agriculturally-oriented character. A windmill tower, buildings that remind us of switching station buildings, water tanks, etc. may be used as vertical structures to signal this node and the approach to the Town Centre core.

This open urban space is proposed to be developed over the right-of-way of the present northern access to the Cloverdale Town Centre once the new intersection at Highway No. 15 and 58 Avenue diversion is built.

A protected playground area (enclosed and slightly below the sidewalk level) and Sunday vendor kiosks may also be considered at this location (See Map No. 12).



Southward view toward the Clover Valley Junction as the northern window of the Town Centre. The windmill is an urban landmark on the Highway and at the northern end of Station Walk.

g) Clover Place

This new development area bridges the gap between the present Town Centre area and the Cloverdale Fairgrounds (and the community facilities proposed north of 60 Avenue along the Highway No. 15 frontage). This area allows for the commercial extension of the Town Centre to the north and provides opportunities to develop commercial uses that do not necessarily have a continuous street frontage (See Map No. 12 for design and character recommendations). Residential/commercial mixed- use developments are also feasible in this area.

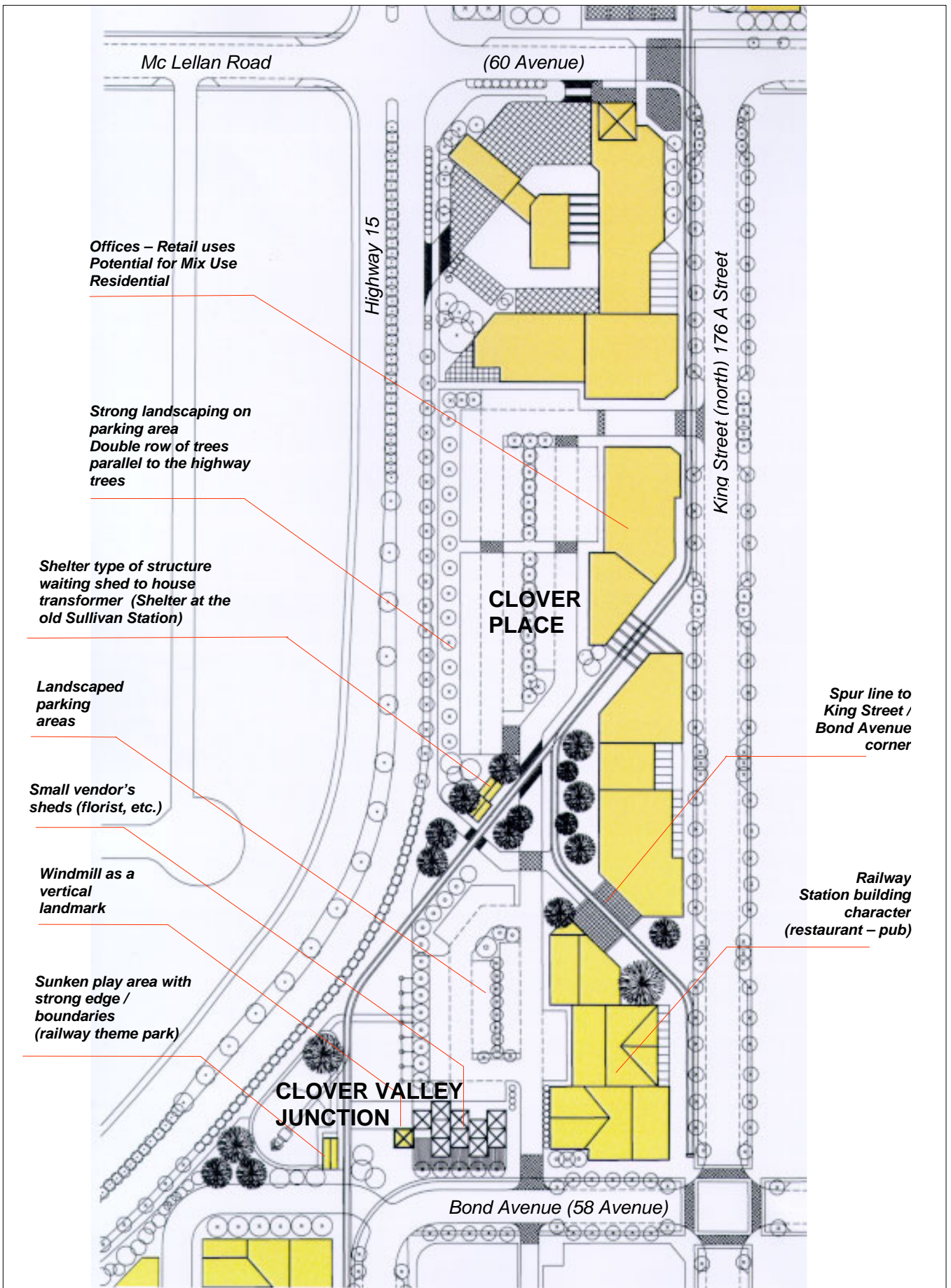
Parking areas along the highway frontage provide the opportunity for formal tree planting and landscaping at the edge to channel long views to the south into the Clover Valley Junction and into the Town Centre.



Building frontage along King Street is less continuous. Parking is located along the highway frontage with extensive formal landscaping that focuses on the Town Centre as the highway turns.



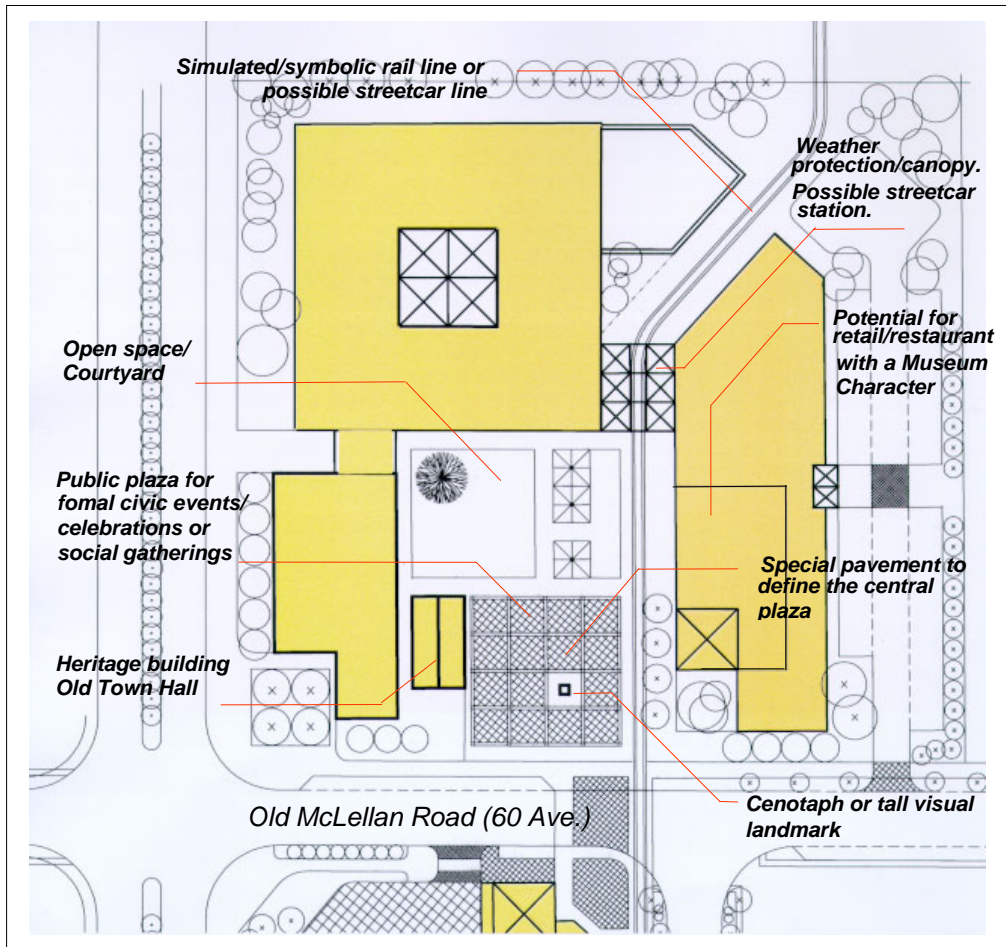
The railway tracks in the pavement meander through this area as a spine that ties the various buildings together and links this new development area to the old Town Centre core. A mix of railway and agriculture (and rodeo) related images are appropriate in this area of the Town Centre.



Map No. 12 – CLOVER VALLEY JUNCTION and CLOVER PLACE

h) Discovery Square

This is the main urban space/destination point that anchors the northern end of the “Station Walk” and defines the gateway to the recreational community facilities and the Fairgrounds north of 60 Avenue. This node is developed around a formal urban plaza where many local civic ceremonies will take place (potential location of the cenotaph). It is proposed that this space be well defined on at least three sides by buildings, including the Museum and Archives and other exhibition facilities, and be open to the south toward the Town Centre.



Map No. 13 - DISCOVERY SQUARE

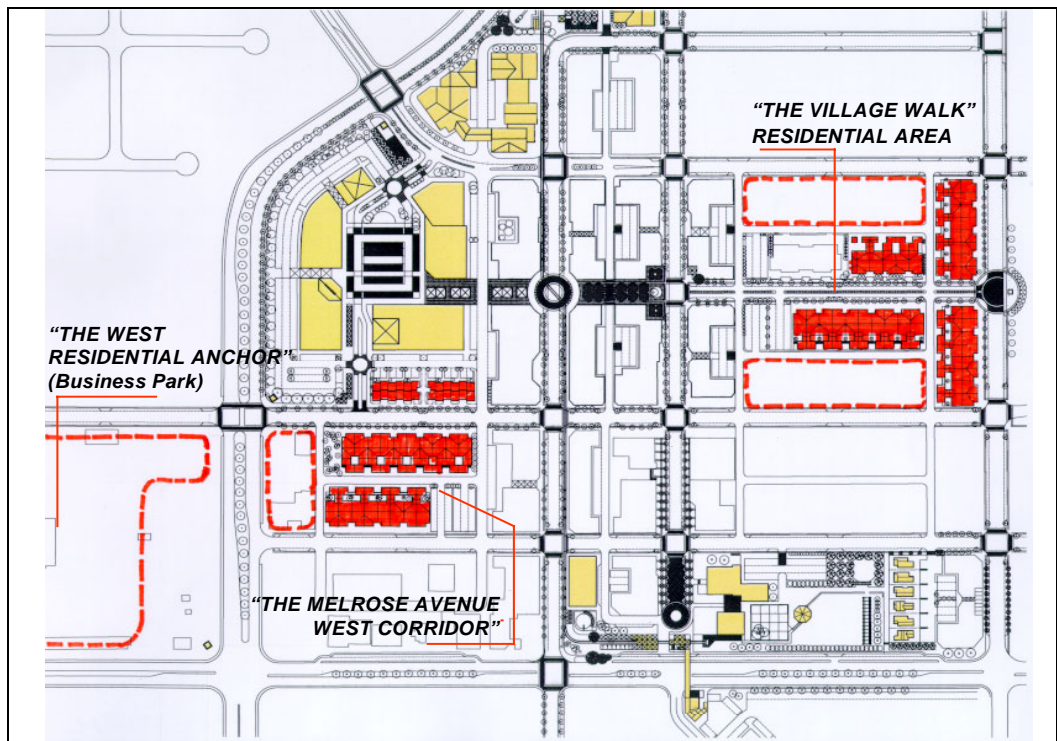
As the northern anchor of the Town Centre, this node is also the ending of the railways tracks (its extension to the north to the Cloverdale Amphitheatre is recommended). Development of this node could include new or historical buildings, or a combination of both, to celebrate the rich heritage of the area while also inviting exploration into the future. A formal tree planting node (The Orchard) is proposed at the corner of 60 Avenue and Highway No. 15 to establish a landmark.

3. Residential Developments In or Near Town Centre Area

Compact and more attractive residential developments are proposed in and near the Town Centre core to provide on-going pedestrian activity and to reinforce the viability of commercial uses.

An overall increase in density could be achieved without losing the single-family character. Narrow single-family lots which allow coach houses with access from the lane, and row housing developments generally represent the desirable residential character for the area.

The following are the proposed residential nodes in the Town Centre:



Map No. 14 - RESIDENTIAL AREAS IN THE TOWN CENTRE CORE

a) “The West Residential Anchor”

This is a proposed medium density multi-family area of three and four storey buildings (depending on soil conditions) on the west side of Pacific Highway (Highway No. 15) between Highway No. 10 and 57 Avenue. It is proposed that a portion of this residential node include a commercial/residential mixed-use component toward the northeast corner of the site to achieve a strong visual and functional linkage to the “Clover Valley Marketplace” across the Highway.

Strong landscaping buffering consisting of tree planting and berm is proposed along the Highway No. 10 frontage as a visual and noise screen. A north-south disposition of buildings on the site and side walls of buildings toward the Highway will increase the protection of the residential area from the intensity of traffic.

Buildings along the 57 Avenue frontage should be compatible with and reinforce the existing townhouse residential character of the street. Higher and more compact building volumes on the southern areas of the site would also serve as buffer from the Highway.

This area on the west side of the Pacific Highway may also be developed as a Business park under the current Official Community Plan designation. It would provide employment in the vicinity of the Town Centre core, help its vitality, and would still define the west anchor to the Town Centre. However, residential land uses should be retained along its north side on the 57 Avenue frontage.

b) “The Melrose Avenue West (57 Avenue) Corridor”

A medium density residential area is recommended along this street, from Pacific Highway (Highway No. 15) to “Station Walk” (176 Street). Because of its proximity to the Town Centre core, apartment buildings or row housing are considered appropriate housing forms across from the “Clover Valley Marketplace” (and possibly on the Legion site in the far future).

Small front yard setbacks, direct access from the street to ground level units, and a well defined street edge are important features to be implemented in this residential area to achieve a strong “urban” character. Formal landscaping and tree lined boulevards will identify this residential corridor as one of the main access routes to the Town Centre from the residential areas on the west side of the Pacific Highway (Highway No. 15).

c) “The Village Walk (57A Avenue) Residential Area”

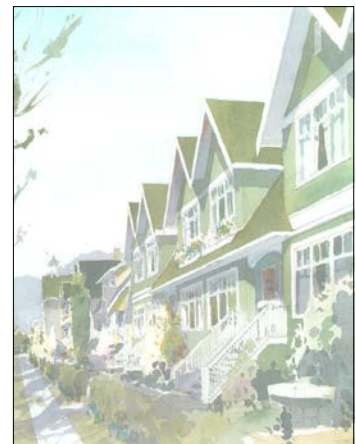
This is the main residential corridor that provides a direct connection between the residential areas on the east side of 177B Street to the Town Centre core and to the “Clover Valley Marketplace”. Appropriate housing forms along this corridor are stacked townhouse and row housing with building massing based on simple, regular shapes with strong gabled roof lines and heights up to three storeys. Residential buildings would emphasize the pedestrian character of this corridor

through compressed front yard setbacks, ground oriented units, articulated frontages, identifiable front entries and human scale detailing.

On-street dedicated parking and rear lanes will reinforce the pedestrian-oriented character of the street.

Detached units with coach houses, duplexes, four-plexes and row houses, at a maximum height of three storeys, are considered an appropriate interface with single family homes in the vicinity of this corridor.

Architectural design and character recommendations for developments located in the residential areas of the Cloverdale Town Centre are provided in Appendix B.



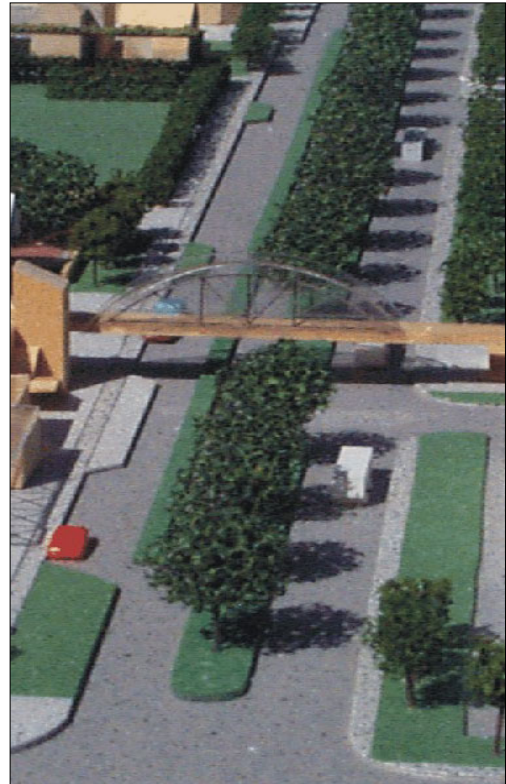
The character of medium density stacked townhouses, row housing and apartment buildings can convey a single family residential atmosphere to the street. The use of gable roof components and clear identification of the front entrance to ground oriented units is a dominant characteristic.

4. Perimeter Routes, Landmarks and Gateways

Perimeter Routes

The Urban Design Concept proposes the special treatment of those portions of the two highways that border the Town Centre area on the west and south. A different character and treatment of the areas contained within the highway right-of-way will announce the crossing of a special area of Cloverdale to the travelers on the highway. Some of the recommended treatments for these regional routes are as follows:

- a) Development of a frontage road and parking lane (see sketch) along the north side of Highway No. 10, between “Main Street” (177B Street) and “Station Walk” (176 Street) to invite and facilitate travelers on the highway to stop, visit the Interpretive Centre and Visitor Information, and stroll through the Town Centre.



- b) A formal planting of trees and double luminaries street lighting placed in the central median on Highway No. 10 will change the character of the highway and will help induce a lower speed of vehicles at the edges of the Town Centre area.
- c) A landscaped median (including trees) and a double row of trees along both sides of Highway No. 15 will be provided. The use of stone is to be used for low retention walls (similar to the low walls around the Senior’s Centre and Library) and for edge definition of sites and public spaces along Highway No. 10 and Highway No. 15.
- d) The use of contrasting colour and texture pavement for the pedestrian crossing at the highway intersections with entrance routes to the Town Centre area (“Station Walk”, Main Street”) to highlight pedestrian priority in the area.
- e) Special pedestrian crossing pavement is provided at the intersection of Pacific Highway with 58 Avenue (the new entrance

to the Town Centre from Pacific Highway). This treatment is also desirable at Pacific Highway and 60 Avenue.

- f) Hardware for the installation of banners to be provided in all lamp posts along these major routes.
- g) Strong colour and textured pavement will be used on the sidewalks on both sides of the Highways.
- h) Stone is to be used for all low retention walls (similar to the low walls around the Senior Centre and the Library), the base of wrought iron transparent fences and for edge definition of sites and public spaces along Highway 10 and highway 15.



Strong and formal edge definition along Pacific Highway

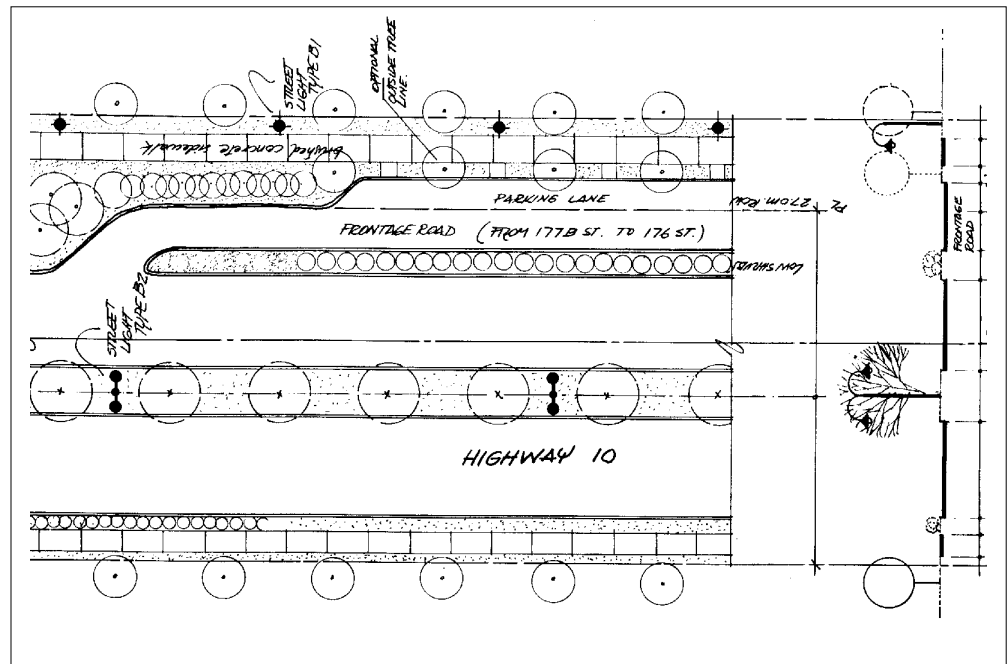


Figure 17. Treatment of the ROW of Highway No. 10

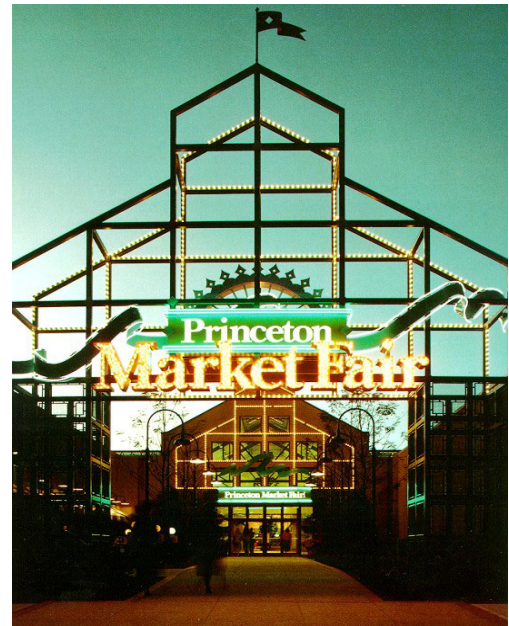
Landmarks and Gateways

The proposed landmarks and gateway elements along the highways (and also at specific locations within the Town Centre) are visual markers that identify the beginning of the Town Centre character area, the entrance to a special place, or unique spaces within the core. They are also reference points for the users of the area.

These visual markers can be unique structures, pieces of public art, free standing quality design character signs, landscaping feature areas or unique pieces of architecture (or architectural feature elements of a building).

There are four gateway points on the highways at the entrances to the Town Centre area: on Pacific Highway at 60 Avenue (north) and at the entrance to the grade separated crossing with the British Columbia Railway (south), on Highway No 10 at 180 Street (east) and at approximately 173 Street (west).

The free standing signs for the “Clover Valley Marketplace” should become unique landmarks at the entrances to the Town Centre from the west and should be artistically designed to reflect the dominant railway/agricultural theme of the Town Centre.



The pedestrian bridge over Highway No. 10 is the most important of all these markers; it defines the entrance to the Cloverdale Town Centre core and is a significant landmark on the highway. This feature element should be the focus of a Public Art process.

A gateway structure is also considered appropriate at the entrance to the “Clover Valley Marketplace” at the western end of Hawthorne Square” and the “Paseo”.

IX. PLAN IMPLEMENTATION

A. General

One of the most important issues arising from the approval of the Cloverdale Town Centre Plan and Urban Design Concept is that of ensuring that the Plan is effectively and efficiently implemented. There are a number of both public and private sector projects along with general street beautification initiatives that together, will allow for the Town Centre to move toward increased economic and social vitality. This section of the report will outline the financial implications of the various projects, identify certain actions needed to see the projects unfold and provide recommendations for implementing the new vision for Cloverdale over time.

B. Financial Implications

1. Street and Circulation Improvements

The physical aspects of the Cloverdale Town Centre Plan represent a major cost for which the City has not at this time allocated specific funds. Innovative funding arrangements involving the private sector, business associations and community groups will need to be investigated to further the eventual redevelopment of the Town Centre. A ballpark estimate of some of the anticipated construction costs to implement the various projects is presented in Table No. 4. Map No. 15 shows the corresponding location of each of the projects described in the Table. These very general cost estimates should be used only for preliminary guidance in planning any construction projects for the Town Centre.

2. Site Development Projects

There are five major development/redevelopment projects proposed for the Town Centre at locations shown on Map No. 15 (Sites 1 to 5). They are briefly described in Table No. 5 along with some comments on possible funding sources and implications.

Table No. 4 – Street and Circulation Improvements

| Project Description <i>(see Map No. 15 for the location of projects)</i> | Estimated Construction Cost ¹ |
|---|---|
| A. Opening and construction of a portion of 56A Street between Station Walk (176 St.) and King Street (176A St.), sidewalks, trees in grates, street lighting. Intersection/pedestrian crossing, pavers on King Street, turn-around, sidewalk improvements, trees in grates, street lighting. | \$1.1 million |
| B. Redevelopment of pedestrian entrance to the Clover Valley Market Place from Station Walk (176 St.), pavement, trees in grates, street lighting. | \$100,000 |
| C. Opening and construction of the pedestrian extension of Village Walk (57A Ave.) to the west, from King Street (176A St. to Station Walk (176 St.). Establishment of the ROW, pavement, seating, planters, trees in grates, street lighting. Pedestrian crossing, parking lane definition, pavers pattern, railway tracks imprint, trees in grates, street lighting. | \$510,000 |
| D. 176 Street - Sidewalk improvements, pavers pattern, railway tracks imprint. Sidewalk construction, parking lane definition, trees in grates, street lighting. | \$945,000 |
| E. 176A Street - Sidewalk improvements, parking lane definition, pavers pattern, trees in grates, street lighting, rear lane improvements. | \$585,000 |
| F. 57 Avenue - Intersection/pedestrian crossings, pavers, sidewalk improvements, trees in grates, street lighting. | \$575,000 |

¹ Does not include land acquisition costs for any street widenings. The unit costs are as follows:

- Collector road upgrades - \$2,500 - \$3,000/linear metre
- Arterial roads - \$3,000/linear metre
- Overpass - \$500,000
- Intersection treatments - \$230,000 each
- Various property purchases - \$270/ sq. m.
- Wood planters - \$6,000 each

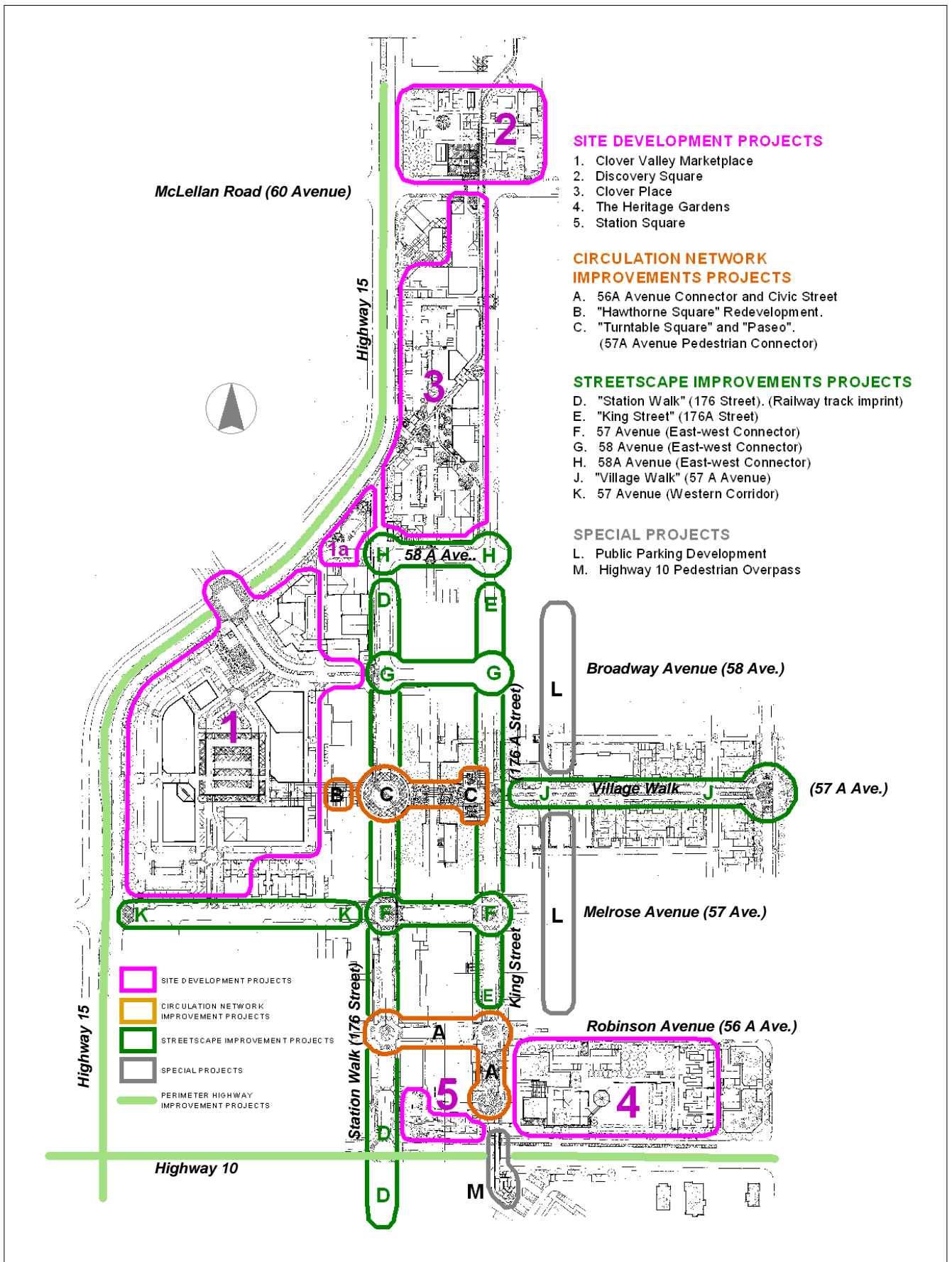
Note: Any utility network upgrades or repairs (storm/sanitary sewers, water, hydro, telephones and cable), traffic signals, property purchases for road widenings, ornamental furniture or sculptures and structural soils are not included in the costs estimated above.

| Project Description <i>(see Map No. 15 for the location of projects)</i> | Estimated Construction Cost ¹ |
|---|---|
| G. 58 Avenue - Intersection/pedestrian crossings, pavers, sidewalk improvements, trees in grates, street lighting. | \$575,000 |
| H. 58A Avenue – Intersection/pedestrian crossings, pavers, sidewalk improvements, trees in grates, street lighting. | \$575,000 |
| J. 57A Avenue - Sidewalk improvements, parking lane definition, sidewalk construction, boulevard trees, landscaped median, street lighting. | \$665,000 |
| K. 57 Avenue - Road and sidewalk improvements, parking lane definition, pavers pattern, trees in grates, street lighting, lane improvements. | \$380,000 |
| L. Public parking lot development (7,169 sq. m. @ \$60/sq. m.) | \$480,000 |
| M. Pedestrian Overpass on Highway No. 10, stairs, elevator. | \$550,000 |
| TOTAL ESTIMATED COST | \$7.04 million |

Table No. 5 – Site Development Projects (see Map No. 15 for project location)

| Project | Funding Implications |
|--|--|
| Site 1. Clover Valley Market Place | The City currently leases out this mall, but may sell it in the near future. The redevelopment project will be undertaken by a developer via an agreement with the City, and most of the improvements, including road and sidewalk construction will be part of the redevelopment of the Mall. |
| Site 2. Discovery Square | The square could be constructed in conjunction with the Learning and Discovery Centre and has potential also to be a public/private partnership. |
| Site 3. Clover Place | The redevelopment of this area will occur over time and improvements will be undertaken by developers in conjunction with new commercial development projects. |
| Site 4. The Heritage Garden and Mixed Use Heritage Home Enclave | These lands are owned by the City (Senior’s Centre, Library, parking). There is potential for this project to be undertaken in stages with contributions from seniors and community groups. Potential revenues may be obtained from the heritage home component where businesses may locate. |

| Project | Funding Implications |
|-------------------------------|---|
| Site 5. Station Square | This project involves a road closure and land acquisition. Estimated construction costs are shown in Table No. 4 above. This will likely be a long-term project and there is potential for community and business groups to participate given its excellent exposure on Highway No. 10. |



Map 15. LOCATION OF IMPROVEMENTS AND DEVELOPMENT PROJECTS

C. Major Contributing Projects

There are a number of major projects “in the works” that will provide a significant economic boost to the local Town Centre economy, are consistent with the new vision for the Town Centre and will hopefully act as a catalyst for increased investment and interest in the Town Centre in the short term. These major projects are briefly described below.

1. Clover Valley Market Place, Hawthorne Square and Clover Valley Junction

The impending redevelopment of the Cloverdale Mall located on the west side of the Town Centre adjacent to Highway No. 15, is expected to occur as early as 2001. This new project identified in the new Town Centre vision, as “Clover Valley Market Place” will certainly “kick-start” the cycle of redevelopment required to turn Cloverdale around. As a major market place anchor and westerly terminus of the Town Centre, this private sector redevelopment project will also prompt the construction of the new intersection/access into the Town Centre on 58 Avenue from Highway No. 15 (enabling better and more direct vehicular and pedestrian access to the Mall and to the core of the Town Centre). The redevelopment of the Mall will also enable the initial redevelopment of Hawthorne Square, will help to establish a design for Turntable Square, and set the tone of development for future improvements for the rest of the Town Centre. The closure of the current access to the Town Centre may also be realized at this time permitting the implementation of the “Clover Valley Junction” public open space as a gateway into the Town Centre from the north.

2. Surrey Learning and Discovery Centre

Although funding has not been established, there is a proposal for a 6,000 and 9,000 square metre (64,600 – 96,900 square foot) Learning and Discovery Centre to be located at the northern terminus of the Town Centre (north-east corner of 60 Avenue and 176 Street). This facility is a significant capital project and a priority of the Parks, Recreation & Culture Department. It would be a major tourist attraction and certainly help to “kick-start” the desired evolution of the Town Centre.

D. Improvements Through Private Development Proposals

As new developments proceed in the Town Centre, street improvements will be undertaken in conjunction with rezoning or development permit approvals. This

will be co-coordinated through servicing and road improvements specified in servicing agreements (which are a usual development requirement for all subdivision and rezoning applications). To ensure general compliance with the guidelines in the Cloverdale Town Centre Plan, Surrey's Subdivision and Development By-law will be amended to include the special designs and construction materials for street improvements generally identified in this Plan.

For example, the proposed new pub/restaurant at the northwest corner of 58A Avenue and 176A Street has incorporated the design principles illustrated in this planning document. The pub/restaurant development may also lead to the eventual redevelopment of the rest of the block northward which is intended to be commercial north to the Fairgrounds, Learning and Discovery Centre and Discovery Square.

E. Community Projects

To contribute to and expedite the new vision for the Cloverdale Town Centre, it will be necessary for many of the urban enhancement projects to be initiated and funded by the community. Projects could be facilitated through the Cloverdale Business Improvement Area Association and other groups, and could include such initiatives as public art and street beautification.

It is noted that the existing horse drawn carriage rides which operate during special events, along with the various fairs and celebrations in Cloverdale lend excitement and enthusiasm for a revitalized Town Centre. These types of activities, which should occur all year-round, are crucial to the future success of the Town Centre.

F. Land Acquisition and Exchange ²

The Plan for the Town Centre identifies several projects which involve the future acquisition of certain privately owned lands and the sale, development, exchange and redevelopment of existing City-owned lands within the Town Centre. The properties affected by the proposals (and ownership) are identified on Map 16. Tables No. 6 and No. 7 indicate the proposed use and comments respecting each of the sites affected by the Plan.

The financial strategy associated with implementing the Cloverdale Town Centre Plan attempts to ensure that the land acquisition costs are offset by the revenues obtained from the sale and redevelopment of City-owned land. A preliminary analysis of the acquisition costs and revenue from the sale of City land indicates that revenues may generally surpass the costs of the proposed land

² Note that all values referred to are preliminary and used for discussion purposes only.

acquisitions. However, this does not include the cost to acquire new parking lots (shown in the shaded rows in Table No. 7). Many of privately owned sites identified for possible acquisition are proposed to be purchased over the long term for parking purposes. Some existing City parking lots (Sites D & H) are located in the commercial core and are proposed to be sold for commercial redevelopment (and replaced by other parking areas adjacent to the commercial streets). The costs of purchasing and operating the new parking areas will be managed through the off-street parking program (which currently operates like a utility) for the Cloverdale Town Centre. Funds may also be derived from the Off-Street Parking Reserve Fund.

Two privately owned small properties on 176 Street (Sites 4 and 5) are crucial to implementing the main east-west connection (Farmer's Walk) and Turntable Square and should be a priority for acquisition. There is currently a small office/retail building on the properties (it is located upon both). A strategy should be put in place for the City to eventually acquire these sites.

The properties located at 17705 Highway No. 10 and 17704 - 56A Avenue are to be used as public open space (portions of Site M). These properties have been identified as surplus to the City's requirements and if they are to become open space, the transfer of funds to the City Land Sale Account may be required.

Table No. 6 - City-Owned Land

| Site | Owner | Proposed Use | Comment |
|--------|-------|-------------------------------------|--|
| Site A | City | Discovery Square | Possible site of Learning & Discovery Centre |
| Site B | City | New Road | Transfer of funds to Land Sale Account |
| Site C | City | Clover Valley Market Place (Mall) | Sale not confirmed; proceeds go to Land Sale Account |
| Site D | City | New commercial | Existing parking lot to be sold & redeveloped |
| Site E | City | New commercial | Existing parking lot to be sold & redeveloped |
| Site F | City | New commercial | Existing parking lot to be sold & redeveloped |
| Site G | City | New commercial | Existing parking lot to be sold & redeveloped |
| Site H | City | New commercial | Existing parking lot to be sold & redeveloped |
| Site J | City | Parking | Existing City parking lot to remain |
| Site K | City | Connecting road | No change to intended use |
| Site L | City | Connecting road | No change to intended use |
| Site M | City | Public open space, civic facilities | No acquisition required |

The properties located at 17526, 17530 and 17532 - 58A Avenue (Site B) will be required for road purposes. They are owned by the City and the transfer of funds received in connection with these lots may be contributed to the Land Sale Account.

The plan proposes that the properties at 17635 - 58 Avenue and 5819 - 176A Street will be redeveloped for commercial uses (Sites E, F and G). These properties are also owned by the City – there is a small recreation centre and an older home on one of the properties and the others are vacant. If these uses were relocated or removed, these lands would be identified as surplus.

The City also owns the Cloverdale Mall and the sale of this site to a private developer is pending.

In summary, a land acquisition strategy should be developed to support the acquisition of the lands required for significant components of the Plan.

Table No. 7 – Privately Owned Land

| Site | Owner | Proposed Use | Comment |
|-------------|--------------|--------------------------------|--|
| Site 1 | Private | Parking | Paid for through parking program |
| Site 2 | Private | Parking | Paid for through parking program |
| Site 3 | Private | Parking | Paid for through parking program |
| Site 4 | Private | Farmer's Walk | Existing building on Site 4 & 5; crucial acquisition; priority |
| Site 5 | Private | Farmer's Walk | Existing building (see above) |
| Site 6 | Private | New commercial & Farmer's Walk | Farmer's Walk acquired through new commercial development (currently a private parking area) |
| Site 7 | Private | Parking | Paid for through parking program |
| Site 8 | Private | Parking | Paid for through parking program |
| Site 9 | Private | Parking | Paid for through parking program |
| Site 10 | Private | Parking | Paid for through parking program |
| Site 11 | Private | Parking | Paid for through parking program |
| Site 12 | Private | Parking | Paid for through parking program |
| Site 13 | Private | Parking | Paid for through parking program |

| Site | Owner | Proposed Use | Comment |
|----------------------|---------|------------------------------------|--|
| Site 14 | Private | Station Square | Long term acquisition only; existing parking lot and restaurant; could be incorporated later with site redevelopment |
| Site 15 | Private | Station Square | Small site |
| Site 16 | Private | Station Square | |
| Site 17 | Private | Station Square | |
| Site 18 ³ | Private | Heritage Garden & business enclave | Long term acquisition possibility; use should be consistent (heritage homes with businesses) |

G. On-going Infrastructure Upgrades

In conjunction with the City’s normal road and utility upgrade program, certain street improvements may be undertaken in accordance with the Urban Design Concept as the infrastructure in the Cloverdale Town Centre is repaired or replaced.

H. Surrey Town Centres’ Project “Implementation” Team

Toward ensuring the on-going planning and implementation of the Cloverdale Town Centre (and the other Town Centres), it is recommended that a City Implementation Team be established under the direction of the General Managers of Engineering, Planning & Development, Finance and Recreation & Culture and the Economic Development Manager. It is proposed that this team explore ways by which improvements, beautification and redevelopment in the Cloverdale Town Centre and in Surrey’s other four Town Centres can be efficiently managed, promoted and prioritised to enhance their economic sustainability and ability to attract new business while expanding the City’s overall commercial tax base. The team could examine City budgets and work programs, identify funding sources and generally organize expenditure and implementation over the short and long term of the various works in Surrey’s Town Centres. The Cloverdale Business Improvement Area Association and other appropriate community groups may also participate in these exercises as required.

³ This is intended to be included in the redevelopment of the adjacent public facility, but is considered to be a long term acquisition, depending on the final design of the redevelopment project.

I. RECOMMENDATIONS

Implementation of the various initiatives in this report requires action on the part of various parties. The recommendations to City Council in connection with these required actions are highlighted below:

- a) Approve the Cloverdale Town Centre Plan and Urban Design Concept as a means for directing and managing the design, development and revitalization of the Cloverdale Town Centre;
- b) Amend the Cloverdale Local Area Plan by including the Cloverdale Town Centre Land Use Plan and Urban Design Concept;
- c) Authorize staff to process applications to amend the Official Community Plan (Land Use Designation Map) in conjunction with rezoning applications, to implement the Cloverdale Town Centre Plan;
- d) Authorize staff to draft amendments to Surrey's Subdivision and Development By-law to identify the road design, tree planting and other applicable standards generally set forth in this report;
- e) Direct the General Managers of Engineering, Planning & Development, Finance and Recreation & Culture and the Economic Development Manager to explore ways by which improvements, beautification and redevelopment in the Cloverdale Town Centre and in Surrey's other four Town Centres can be efficiently managed, promoted and prioritized to enhance their economic sustainability and ability to attract new business while expanding the City's overall commercial tax base;
- f) Authorize City staff to continue to network with and support the Cloverdale Business Improvement Association in its endeavours to promote and attract business and to achieve the revitalization initiatives contained in the Cloverdale Town Centre Plan and Urban Design Concept; and
- g) Direct staff to bring forward a supplementary report regarding land acquisition issues in the Town Centre, particularly with respect to strategic lands required to implement the significant components of the Town Centre Plan.

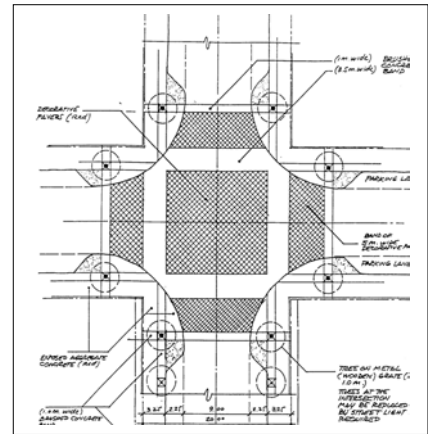
APPENDIX A

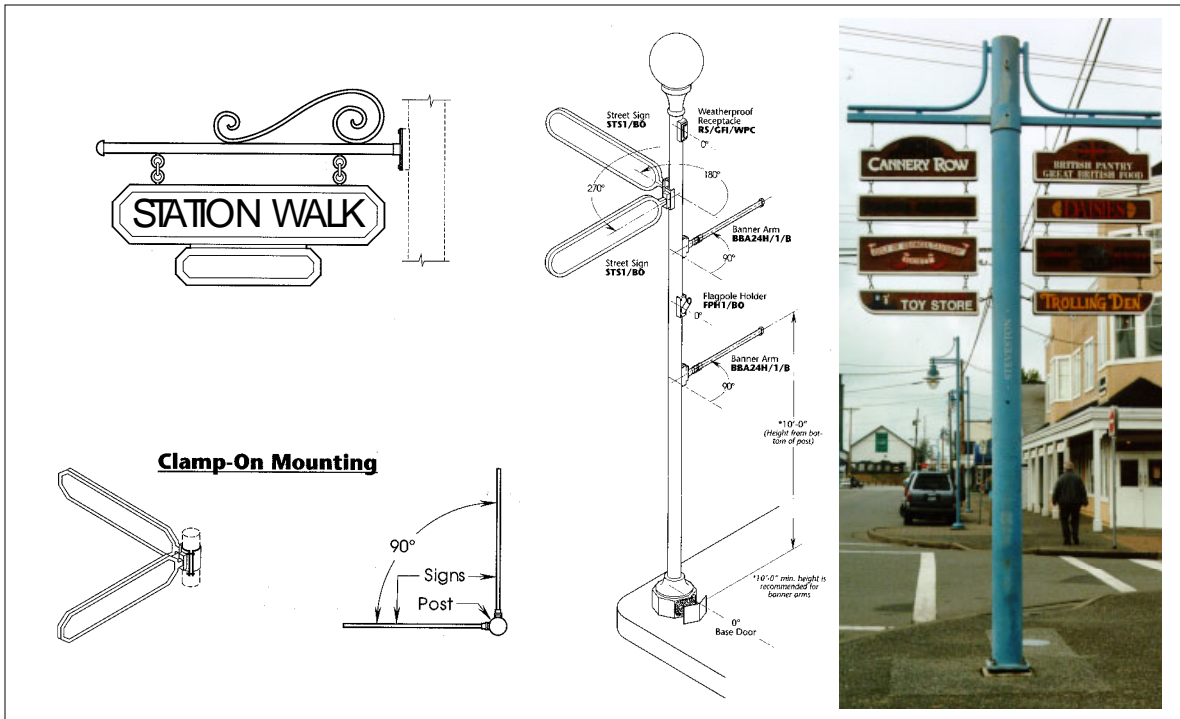
DESIGN AND CHARACTER RECOMMENDATIONS FOR THE TOWN CENTRE CORE STREETS

1) Design and Streetcape Character Recommendations Applicable to All Streets in the Town Centre

Streets in the Town Center core should consider the following (Sketch A provides some images of the desirable general street character):

- a) curb extensions at the intersections;
- b) pavement of contrasting colour and texture at street and lane pedestrian crossings;
- c) on-street parking provided in clusters separated by landscaped curb extensions;
- d) continuous band of contrasting colour and texture (decorative pavers) alongside the curb. Tree planting and of pedestrian scale street lamps to be placed along this band;
- e) an integrated street name signage and lamp post design, including banners hardware;
- f) use of original street names should be adopted (numbered street names should only be added as a reference);
- g) double luminaries to highlight simulated railway tracks route through new development sites and parking areas of the town centre;
- h) use of the sidewalk as an open book to tell the story of the railway and the agricultural past of Cloverdale (i.e. metal plaques may be imbedded in the pavement or placed on the front of buildings at significant sites);
- i) provide bollards at street and lane pedestrian crossings, and around Turntable Square, Hawthorne Square and on the turn-around at the end of King Street (176A Street); and
- j) tree planting at a relatively close spacing (maximum of 10 metres apart).



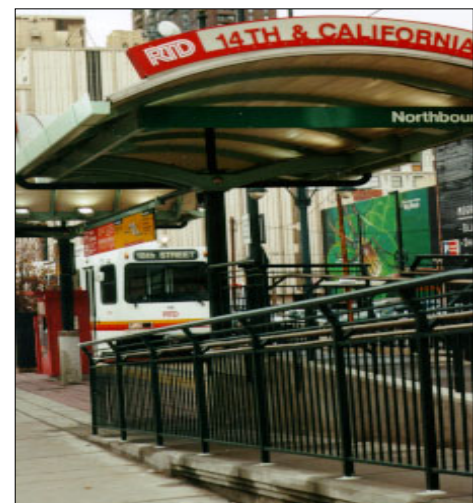


Sketch A

2) Design and Streetscape Character Recommendations for “The Station Walk” (176 Street) and “King Street” (176 A Street) See Figures 1 and 2 for recommended treatment of the street ROW along the north-south axis.

The role played by the railway in the evolution of the town site should be highlighted by the street furniture, the treatment of sidewalks, and buildings along the Station Walk (176 Street) and “King Street” (176A Street). The following guidelines should be considered as street improvements and revitalization occur in the Town Centre:

- a) steel rails to be imbedded in the sidewalk and expansion joint pattern that reinforces the image of a railway track to follow the old alignment of the Great Northern Railway line along “Station Walk” (176 Street);
- b) the chronological story of the railway in Cloverdale could be told along the simulated railway tracks using bronze plates imbedded in the sidewalk (or some similar approach);
- c) metal elements and architectural/structural details related to old railway related buildings or structures are recommended for canopies, supporting elements (brackets), and covered breezeways, etc.;
- d) metal is recommended for tree grates, railings, benches, and similar components of the streetscape;
- e) use wall mounted lamps of a design that bring images associated with lanterns and associated paraphernalia;
- f) bus shelters and bus waiting areas to use designs associated with old railway station platforms and canopies;
- g) street nameplates that include a railway related symbol in their design and metal lamp posts with brackets for banners should be considered along the main streets;
- h) signs along “Station Walk” and “King Street” should preferably be wrought iron projecting signs and use flood or indirect lighting for illumination.



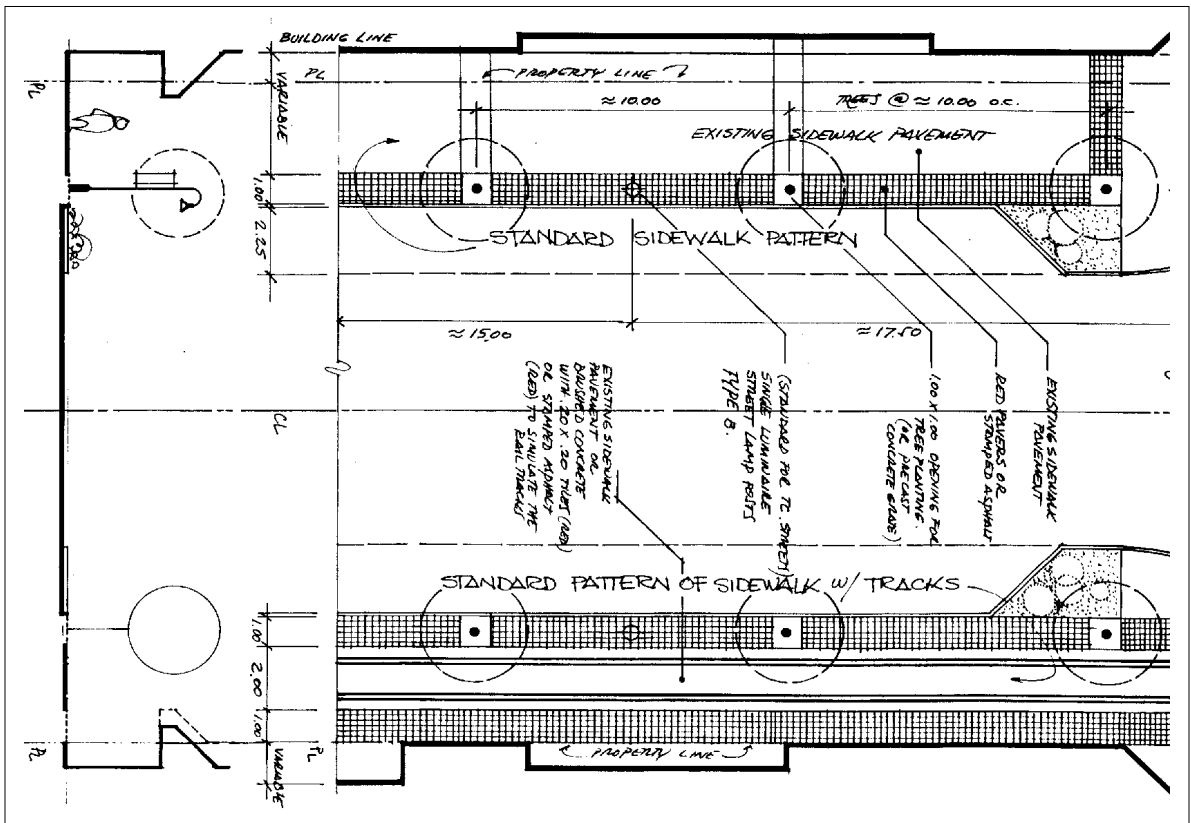


Figure No. 1 - RIGHT OF WAY AND SIDEWALKS. "STATION WALK" and "KING STREET"

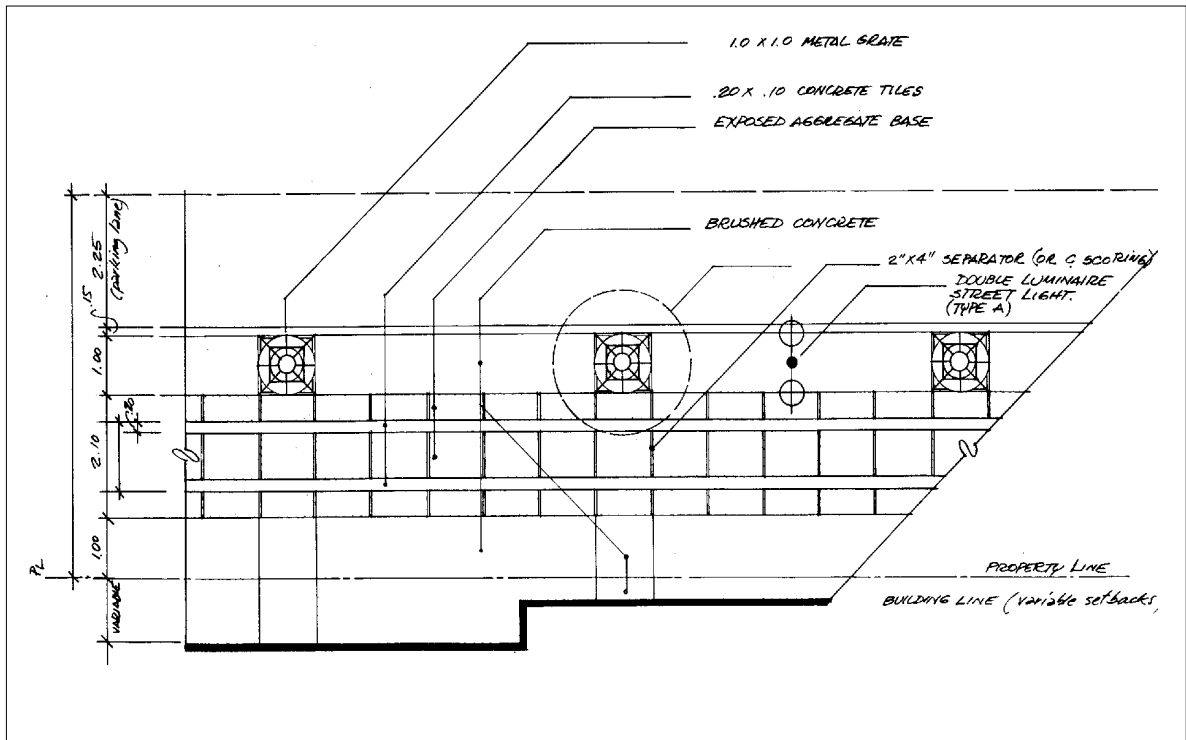


Figure No. 2 - DETAIL OF SIMULATED RAILWAY TRACK (OR ACTUAL RAILS) IN SIDEWALK

3) Design and Streetscape Character Recommendations for “The Village Walk” (57A Avenue) (For design recommendations regarding the desirable architectural character for this residential street see Appendix C).

Reinforcement of a small town residential street character should be generally reinforced by the use of wooden details and a strong street orientation of the buildings. Some guidelines to achieve this general character are as follows:

- a) double row of trees on both sides of the street (on the street right-of-way and on private property) and a landscaped median to reinforce a pedestrian oriented “village” character and slow moving vehicles;
- b) street nameplates should include an agriculture/farming related symbol in their design;
- c) human scale street lighting with flower basket hangers;
- d) grass boulevards, instead of decorative pavers provided alongside the curb of the on-street parking lane;
- e) use of three board fences for enclosures, fences and edge definition of public parking areas exposed to views from the street;
- f) wooden trellis structures along the edges of public parking lots to achieve certain continuity of street frontage;
- g) street furniture (benches, planters, parking lots edge definition, garbage containers, low retaining walls, etc.) should use wood as the dominant material;
- h) use of agricultural motifs in street furniture (e.g., half- barrel planters) is recommended to tie with the Cloverdale’s agricultural past and to reinforce the identity of this axis; and
- l) large wooden planters/seating and double luminaries located along the “Paseo” portion of the “Village Walk”



APPENDIX B

DESIGN AND CHARACTER RECOMMENDATIONS FOR THE TOWN CENTRE CORE RESIDENTIAL AREAS

There are a number of simple guidelines that should be considered in the design of residential developments in the Town Centre area. The preferred housing type in residential areas in or in the proximity of the core is row-housing and stacked townhouses with reduced front yard setbacks. Apartment buildings in the area should have a strong street orientation and character.

To increase overall residential densities, redevelopment of single family areas in the vicinity of the Town Centre should consider narrow lot single-family units with “coach houses”, duplexes and four-plex housing types.

General design recommendations for residential areas are as follows:

- a) front setbacks to be between 3.0 m. and 5.0 m. and parking should be accessed from the lane to ensure continuous street frontage;
- b) to increase privacy, the finished grade of the main floor of homes should be not less than 0.60 metres above the sidewalk level;
- c) there should be a clear delineation of public and private space; low stone walls or in combination with low transparent wooden fences are recommended;
- d) individual front entries to lower level units should be strongly defined and should relate to the streets;
- e) extended porches and recessed entries should be used to articulate facades and reinforce an unified residential character;
- f) verandas and porches with sloping roofs toward the street should be dominant features on residential street frontages;
- g) veranda posts (single or double) could include braces or decorative brackets;
- h) windows should be of a simple configuration; large picture windows are not considered appropriate to the desired character for the



Town Centre area; groupings in bays are strongly encouraged

- i) windows should have dominant vertical lines that include muntins and mullions;
- j) trimming of windows should be consistent throughout all the building elevations;
- k) window planters are recommended to create a festive and friendly face for the dwelling units facing the street;
- l) a strong primary roof form (preferably gables) should address the street, with secondary roofs (gables) to visually support the primary roof;



- m) minimum roof slope to be 8:12; steeper roofs and attics with dormers are preferred;
- n) horizontal planks should be used as a dominant siding material and the sidings should have strong colours to contrast with the trim around fenestrations;
- o) building and roofing materials should express a high level of craftsmanship and reflect the local heritage and regional climate; and
- p) detail elements on roofs may include gabled dormers, ventilation cupola, wind vanes and similar features.



APPENDIX C

CLOVERDALE TOWN CENTRE BUILDING DESIGN GUIDELINES

1) Introduction

These guidelines are intended to reinforce the overall character objectives of the Urban Design Concept Plan by creating an identifiable, pedestrian-friendly Town Centre with a human scale and a strong local flavour associated with the railway and agricultural heritage of the town site.

The guidelines are applicable to all new developments and improvements to existing buildings located within the Town Centre area and its immediate interface.

For consistency of character and to reinforce the overall image of the area, it is important to increase awareness of the feature elements, which formed part of the architecture of buildings found in Cloverdale during the first quarter of the 20th century. Potential use or a contemporary interpretation of these elements is expected in the design of new buildings in the area.

2) Architectural Heritage/Historic Background

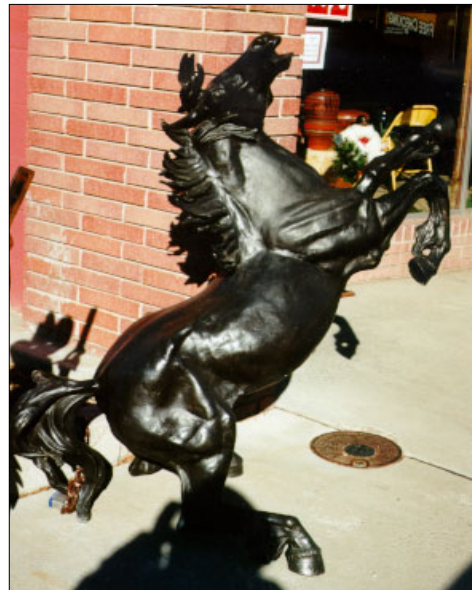
(See inventory of small town “Heritage Feature Elements” which identify architectural features to be re-interpreted in new development proposals.)

Historic buildings in the Town Centre were characterized by simple building volumes and the use of traditional materials enhanced by wood detailing. Some of the key architectural characteristics of the historic buildings are:

- a) simple building forms, materials and detailing;
- b) most commercial buildings in the Town Centre are either wood frame construction or brick (masonry), with cornices capping a simple rectangular façade;
- c) the ground floors are over height (i.e., 10 - 12 feet);
- d) storefronts consistently span across the width of the store and consist entirely of panelled glazing;
- e) there are cornice lines at the top of the parapet and sometimes above the storefront windows; there are heavy wood lintels, sills and trims;
- f) the earliest buildings included a large false front built above the ground floor which hid a gable roof behind; in some cases, a true second floor for storage or living quarters was incorporated which had vertically oriented double-sash windows punched through the solid wall; and
- g) doors into buildings were typically located in the centre of the building, and recessed in a covered alcove; some buildings incorporated porches which extended across the width of the building.



- Strong horizontal cornices and trims
- Overheight ground floor and mullioned windows
- Some false fronts with gables behind
- Porches with posts
- Recessed entries





- Heavy wood lintels, sills, trims, and brackets
- Painted and projecting signs
- Vertical windows on upper floors
- Entrance door for retail store centrally located in the store front.
- Storefronts span across width of the CRU's
- Panelled glazing



3) Design Guidelines for Developments Along “The Station Walk” and “King Street” Commercial Areas (excepting the “Clover Place” development site)

In addition to the use of imaginative and lively designs which bring images of railway related structures, some of the following components should be considered in new buildings and their immediate urban context (streetscape):

a) Site Layout and Parking Areas

- i) buildings should preferably be constructed up to the front property line; front setbacks should not be more than 2.0 m. to maintain the existing urban character defined by the continuity of commercial frontage;
- ii) to create small plazas as gathering places and to facilitate social interaction, substantial setbacks should be provided at specific locations along the two commercial streets as indicated in the Urban Design Concept Plan;
- iii) provide direct access from the street to narrow frontage retail commercial units. In the case of a large, ground level, single commercial tenant provide various access points to ground level;
- iv) corner buildings should have access to the ground floor from both streets;
- v) retail and commercial frontages should continue around the corner to minimize exposure of the parking areas toward side streets. Use post and beam structures, trellises, colonnades, or similar light structures to define edges and partially screen views to parking areas on side streets;
- vi) off-street parking areas should be provided at the back of the buildings and have access from the rear lane. One pedestrian linkage (breezeway) from the lane should be considered at mid-block to provide pedestrian access from shared parking areas to the street;
- vii) parking areas should be broken into small clusters using raised curbs and landscaped islands;



- viii) the number of access points to parking areas should be minimized, and combined or shared entrances with other properties should be considered;
- ix) the edge of parking lots along the lane should be well defined; the use of wrought iron fences combined with landscaping (including trees) or similar strong types of edge definition is recommended along all lanes;



- x) lighting in parking lots should have a pedestrian/human scale with downward oriented light fixtures. Intensity and location of lighting for the parking areas should retain a pedestrian scale and should not spill on adjacent properties;
- xi) decorative pavers, stamped coloured concrete/asphalt or other combinations should be used to break the parking area into smaller clusters, identify pedestrian routes and define the threshold between the public lane and the parking areas; and
- xii) Utility entrances, garbage disposal areas, and other service elements should be located at the back of the building and should be properly enclosed. Garbage container enclosures should be built of the same materials and finished with the same quality as the building.

b) Building Form and Materials

The design of buildings located in the Town Centre commercial areas should consider every opportunity to use the simple forms, scale, proportions and details found in old buildings associated with the operation of the railway (water towers, water tanks, railway depots, round-houses, switching station buildings, passenger waiting platforms, station canopies, etc.). The following should be considered:

- i) new buildings should blend with the existing built context; the height and scale of adjacent structures should be respected and a gradual transition should be achieved at the interface line;
- ii) long, uniform, unarticulated building frontages should be avoided. As a reference, it is recommended that the width of an unbroken building front should not be more than 8-10 metres (26 – 33 ft.); long building fronts should consider an offset, pattern or articulation sufficient to provide a strong shadow line that emulates a series of 8-10 metre-wide (26 – 33 ft.) frontage building modules;
- iii) new buildings should retain the lines of eaves, rooflines, cornices and parapets of existing structures to achieve a proper transition between old and new buildings;
- iv) if gable roofs are considered, they should have pronounced slopes; the use of eave brackets is encouraged;
- v) use of decorative and character elements to provide details to roofs exposed to views. Weather vanes, cupolas, etc. should be considered;
- vi) use of traditional materials such as wood, brick or stucco is encouraged. The use of glazed curtain wall systems, plastic or glazed ceramic (other than for special artistic decorative purposes) is not considered appropriate. Large areas of exposed natural or painted concrete are discouraged;
- vii) materials may vary from building to building to reduce the scale and to provide variation and diversity in the streetscape; however, the number of materials used should be limited within a single building; and
- viii) the colour of fascias, windows and door trim should contrast with the rest of the building; colours in warm tones are preferred as background colours, and contrasting colours should be used to accentuate decorative facade features.



c) Building Facades

Facades should provide a flavour of heritage forms and architectural details. New building facades should consider first floor base lines, large storefronts with recessed entries and vertical orientation of windows on upper floors. It is also recommended that approximately 75% of the ground floor facade be occupied by windows.

Some recommended facade details include:

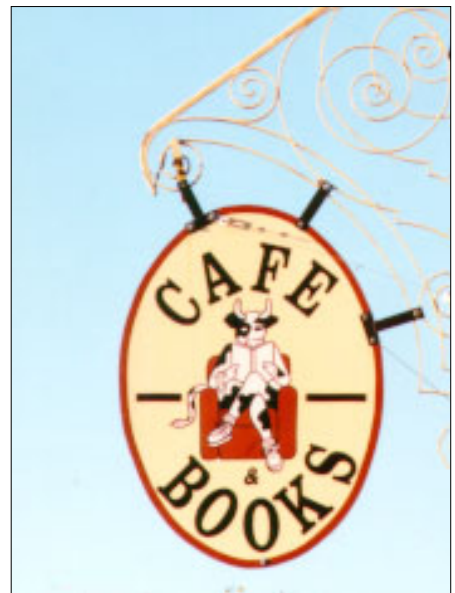
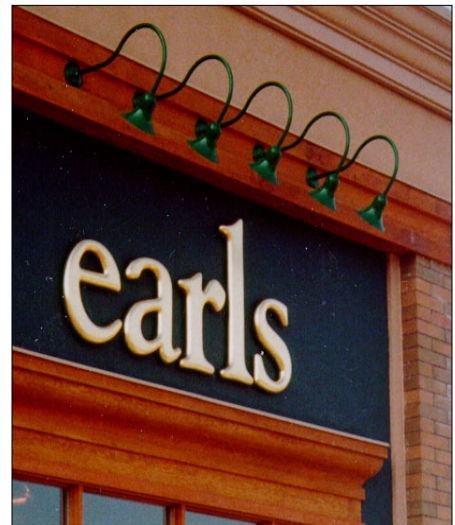
- i) Recessed entry areas for retail units;
- ii) Strong eave lines;
- iii) Solid or glass panels across the facade above the storefront windows (to exaggerate the height);
- iv) Parapet walls with ornamental cornices;
- v) Wide canopies and overhangs (not encroaching into the public r.o.w.) and use of open brackets (wood or metal);
- vi) Wrap around or full width verandas;
- vii) Square, turned base supporting columns and decorative brackets;
- viii) The use of wood panelled doors with inset glass panels in storefronts should be considered.
- ix) Storefronts that include large pane windows regularly divided by muntins and mullions;
- x) Double hung sash windows on upper floors and the use of transom glass;
- xi) Windows with heavy lintel and sill, and wide window casements; and
- xii) Traditional clapboard siding.



d) Signage

Signs in the Town Centre core should be restricted to projecting signs, wooden wall mounted board signs, neon or painted window signs, wall painted signs, canopy/awning signs (excluding backlit awnings), and under canopy signs. Recommendations regarding signage are as follows:

- i) signs in the town centre should become one of the streetscape character's unifying elements and should reinforce the railway and agricultural themes;
- ii) only indirect, flood lighting should be used for sign illumination;
- iii) the form, size, style and location of signs should complement and reinforce the architecture of the building façade. Signage should not obscure or affect important features of the building or its overall architectural integrity;
- iv) rooftop signs are specifically not permitted in the town centre area. Free standing signs that are designed as public art or unique landmarks, and those that contribute to the overall theme could be considered on an individual basis but in general, free-standing signs are not appropriate in the town centre core;
- v) design, details and the subject used in signs should reflect the image and main activity of the business. Consistency of lettering and quality should be more important than its size;
- vi) signs should be used for business / establishment identification purposes only. Signs used for advertising tend to clutter the streetscape and are not appropriate for the image of the town center;
- vii) identification signs are recommended to be limited to one major sign on the principal façade and a minor sign on the secondary façade;

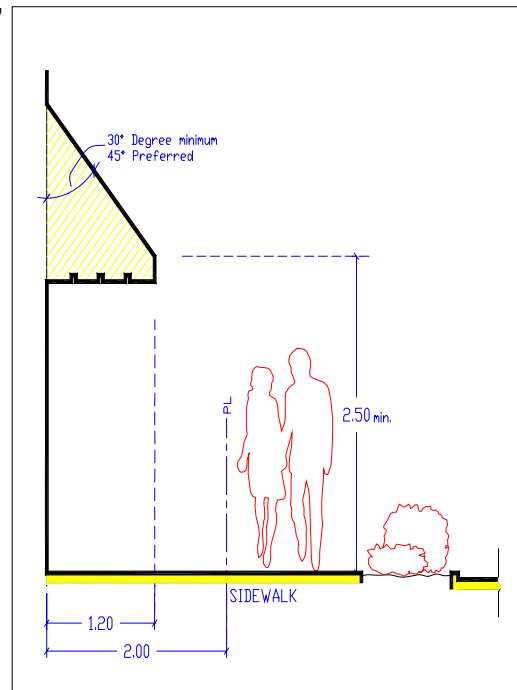


- viii) directional/information signs should be small and discrete in relation to the overall building size; and
- ix) the colour of signs should be compatible with the colour scheme of the building. Although colours that contrast with the building are possible, the overall colour scheme of signs and buildings should be coordinated.

e) Awnings and Canopies

The following guidelines should be consider in regard to these features:

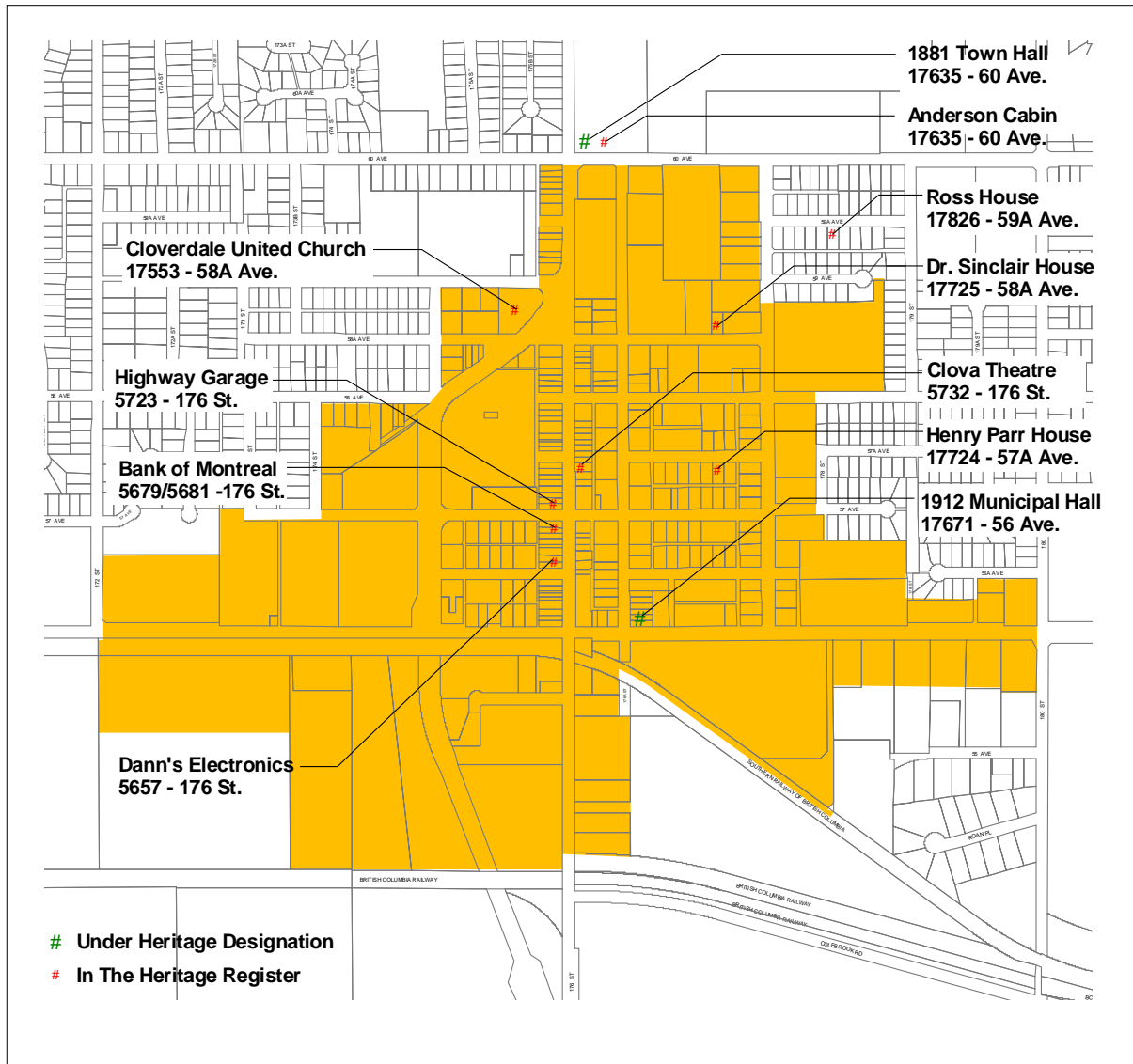
- i) the use of simple, regular sloping awnings and canopies is encouraged;
- ii) canopies and awnings should be opaque;
- iii) to provide weather protection the minimum recommended canopy projection from the building face is 1.20 metres (4 ft.);
- iv) the minimum recommended inclination of canopies is 30 degrees (35 to 45 degree inclination is preferred);
- v) minimum vertical clearence for awnings and canopies is 2.50 metres (8 ft);
- vi) dome canopies/awnings are not considered appropriate in the Cloverdale Town Centre; and
- vii) canopies/awnings should not be used for signage purposes.



Basic design standards for canopies and awnings

APPENDIX D

HERITAGE SITES AND BUILDINGS IN THE TOWN CENTRE



Heritage Designation. A “designated” building, structure or other feature is one that has sufficient heritage significance to be protected by by-law. No alterations can be made to the building without approval from Council.

Heritage Register. The Heritage Register is a formal listing of buildings, structures and other features which have heritage value and character, based on architectural, historical, cultural or natural significance. Some of these have potential to receive “designation” in the future. Placement in the Heritage Register requires a resolution of Council.

- a. **Anderson Cabin** – 17365 – 60 Avenue (Historical). The Cabin was built c. 1872, and as such is documented as the Surrey's oldest remaining building. It features notched corners and chinking. It is preserved as a historical monument to all pioneers, and its crude structure shows how primitive the living conditions were for those pioneers. It was moved to the Museum site in 1982.



- b. **1881 Town Hall** - 17635 - 60 Avenue (Historical): Surrey's first Town Hall, built in 1881 and in use until 1912, originally located across from Christ Church on Old McClellan Road (Five Corners) and now located inside the Surrey Museum in the Fairgrounds. Protected by Designation By-law, 1982, No. 7289.

(No picture available)

1912 Municipal Hall - 17671 - 56 Avenue (Architectural, Historical): Neo-Tudor style consisting of rough-cut stone cladding, masonry and false half-timber. Built in 1912, this second Municipal Hall was in use until 1962. Protected by Designation By-law, 1980, No. 6442.



- c. **Cloverdale United Church** - 17553 - 58A Avenue (Architectural): Pointed arch windows in gothic Revival style, with curved lead work creating an arched lattice pattern. Triplet arrangement in south window includes floral motif, curved lead work in pointed arches. Rectangular nave with intersecting transept, two-storey bell tower with hipped flared roof partially inset into southeast corner.



- d. **The Clova Theatre** - 5732 - 176 Street (Architectural, Cultural): Art Deco style theatre built in 1946 as one of the first modern buildings in Cloverdale. Typical of small town movie houses of the 1940s with a curved facade, vertical sign projection and decorative moulding above the second floor window.



- e. **Henry Parr House** - 17724 - 57A Avenue (Architectural, Historical): Built c. 1912 in a plain Victorian style with symmetrical facade and small covered entry porch with spindle work on columns borrowed from Queen Anne style, a design similar to those found in published plans and mail order packages.



- f. **Ross House** - 17826 - 59A Avenue (Architectural): Bungalow-style featuring shingle siding, low pitched gabled roof with knee braces, partial width front porch with separate gabled roof supported by tapered square columns, stick detailing in gable of front porch. Style of house suggests construction some time between 1920 and 1930.



- g. **Dann's Electronics** - 5657 - 176 Street (Architectural, Historical): Two-storey square house form adapted for commercial use, with asymmetrically located entrance and large display windows as the features presenting a commercial character. Built in 1920 and originally owned by the Royal Bank until 1924; occupied by Dann's Electronics since 1932.



- h. **Bank of Montreal** - 5679/5681 - 176 Street (Architectural, Historical): Neo-classical style featuring cornice with crown mouldings and details on front facade, less elaborate cornice between first and second storey, and symmetrical facade. Stucco covers original brick likely to protect poor quality masonry. Built in 1912 and occupied by the Bank until 1958.



- i. **Highway Garage** - 5723 - 176 Street (Historical): Originally a livery stable built in 1919 and became a garage the following year. Square plain industrial shed underwent major renovations in 1938, adding barrel vault roof and false front facade with opening on facade that contained the fire siren for Cloverdale Volunteer Fire Brigade.



- j. **Dr. Sinclair House** – 17725 – 58A Avenue (Architectural, Historical): Dr. Sinclair was appointed the Municipal Medical Health Officer in 1913, a position he held until his death in 1952. He was well known as a family doctor in Cloverdale and across Surrey.

The house was constructed c. 1900 in American Foursquare Style. Features symmetrical façade with one room protrusion on the west side,



Oriel window and four symmetrically placed dormers. The house was originally located on highway 10, across from the Surrey Co-op, and relocated to its present site after Dr. Sinclair's death.

C013 : Cloverdale Town Centre Land Use Plan and Urban Design Concept

Corporate NO: C013

Report COUNCIL DATE: October 30, 2000

COUNCIL-IN-COMMITTEE

TO: Mayor & Council

DATE: October 26, 2000

FROM: General Manager, Planning & Development

FILE: 2125-203

SUBJECT: Cloverdale Town Centre Land Use Plan and Urban Design Concept

RECOMMENDATION

It is recommended that Council:

1. Approve the attached Cloverdale Town Centre Plan and Urban Design Concept as a means for directing and managing the design, development and revitalization of the Cloverdale Town Centre;
2. Amend the Cloverdale Local Area Plan by including the attached Cloverdale Town Centre Land Use Plan and Urban Design Concept;
3. Authorize staff to process applications to amend the Official Community Plan (Land Use Designation Map) in conjunction with rezoning applications, to implement the Cloverdale Town Centre Plan;
4. Authorize staff to draft amendments to Surrey's Subdivision and Development By-law to identify the road design, tree planting and other applicable standards generally set forth in this report;
5. Direct that staff of the Engineering, Planning & Development, Finance, and Parks, Recreation & Culture Departments, and the Economic Development Manager explore ways by which improvements, beautification and redevelopment in the Cloverdale Town Centre and in Surrey's other four Town Centres can be efficiently managed, promoted and prioritized to enhance their economic sustainability and ability to attract new business while expanding the City's overall commercial tax base;

6. Authorize City staff to continue to meet with and support the Cloverdale Business Improvement Association in its endeavours to promote and attract business and to achieve the revitalization initiatives contained in the Cloverdale Town Centre Plan and Urban Design Concept; and
7. Authorize staff to bring forward a supplementary report regarding land acquisition issues in the Town Centre, particularly with respect to strategic lands required to implement significant components of the Plan.

INTENT

The intent of this report is to provide a summary of the planning process and a brief overview of the attached “Cloverdale Town Centre Land Use Plan and Urban Design Concept”, and to seek Council’s final approval of the Plan and Urban Design Concept.

BACKGROUND

In late 1997, the Cloverdale Business Improvement Association approached Council requesting that the City conduct a review of the land use plan for the Cloverdale Town Centre and that design guidelines be prepared concurrently with this review. The Terms of Reference for this review were approved by City Council in May 1998, and outlined a strategic planning process that defined the study area, the plan preparation process and the content of the Plan. The planning process is now complete and has resulted in a comprehensive land use, design and economic enhancement initiative for the Cloverdale Town Centre.

Currently, the Cloverdale Town Centre is included in the Local Area Plan for Cloverdale. Land uses in the Town Centre have not been the subject of a review since the early 1980’s, and the Town Centre has experienced some difficulties with attracting new business and competing with nearby commercial centers. Four new neighbourhoods in North and West Cloverdale have been approved, and Cloverdale (excluding Clayton) is anticipated to grow from a population of about 25,000 today to about 36,000 within the next few years. It is timely, therefore, to put forward an updated vision and policy framework for the Town Centre to help it further establish itself as a vibrant and economically sustainable commercial destination.

DISCUSSION

Overview of the Planning Process and Public Consultation (Section IV in the attached Plan Document)

Highlights of the planning process, the participant groups and public meetings are summarized below:

- (a) *Cloverdale Town Centre Focus Group:* This group consisted of 18 individuals including residents and business owner/operators and was instrumental in developing land use options, helping staff understand the local business environment and identifying heritage opportunities. The Focus Group was invaluable in bringing local knowledge and business expertise into the planning process.
- (b) *Public Meetings:* There were three public meetings held at stages during the planning process at which staff sought opinions on the future of the Town Centre, administered questionnaires, presented the various proposals and confirmed public acceptance of the final Plan.
- (c) *Inter-Departmental and Inter-Agency Input:* Liaison was maintained throughout the planning process with the Heritage Advisory Commission, the Cloverdale business community, the Engineering Department, the Parks, Recreation & Culture Department and external agencies.
- (d) *Selection of the Preferred Land Use Plan:* Three land use options were initially put forward to the participants and the public. Following public review and an economic analysis, the “mid-sized Town Centre option” (known throughout the process as Option 3) was selected, determined to be the most commercially viable and therefore further refined. It subsequently served as the basis upon which the Urban Design Concept, circulation and linkage concepts and the overall enhanced image for the Town Centre were formulated.
- (e) *The Cloverdale Business Improvement Area (BIA):* The Cloverdale Business Improvement Area (BIA) was originally established in 1995 (by City of Surrey By-law) to improve and promote businesses in the area. The BIA is funded through a rate levied against properties in the Cloverdale Town Centre area that is collected in conjunction with annual municipal taxation. The BIA Executive was an excellent resource and assisted throughout the planning process in ensuring that the proposed plans met the needs and objectives of the business community.

Overview of the Land Use Plan (*Map No. 2 in the attached Plan Document*)

(a) *Main Components of the Land Use Plan*

The main features of the Land Use Plan are summarized below.

- i) The focus of the Town Centre is along the two main streets: 176 and 176A Streets.
- ii) There are limited expansion opportunities for service and highway commercial uses along Highway No. 10.
- iii) There is a strong commercial link between the Town Centre core and the Cloverdale Fairgrounds.
- iv) The City’s Cloverdale Mall site (on Highway No. 15 at the west side of the Town Centre) is well connected with the commercial core and is an integral part of the Town Centre (especially once it is redeveloped).

- v) There is a safe pedestrian access and linkage from the Town Centre to the Clover Square Village site (south of Highway No. 10).
 - vi) Development of a new intersection is proposed at 58 Avenue and Highway No. 15 to substantially improve accessibility and safe pedestrian crossing to the Town Centre from the west (the existing entrance at the north end of 176 Street is proposed to be closed).
 - vii) City lands located east of the Library and Senior's Centre will be developed for public uses.
 - viii) The BCBC works yard site at Highway No.'s 10 and 15 will be redeveloped to provide more attractive land uses such as hotel, ancillary retail, multiple family residential and/or business park along with gateway features.
 - ix) An integrated parks and open space system will provide good linkages to the residential community.
 - x) Convenient public parking facilities will be provided within easy walking distance of the Town Centre.
- (b) *Land Use Policies*

There are a number of land use policies contained in the attached document to guide development in the Cloverdale Town Centre. These policies are targeted at leading Cloverdale toward a healthy and viable future as a thriving commercial centre. The policies encourage certain directions in the areas of commercial and industrial development, residential development, vehicular and pedestrian circulation, community amenities and heritage.

(c) *BCBC Lands – Highway No. 10 and Highway No. 15*

There is an existing industrial property (the Highway's works yard) located at the north west corner of Highway No. 10 and Highway No. 15 that could be considered for an alternative land use. Because of the site's close proximity to the Town Centre and the expressed public desire to have a more attractive land use at this corner, one option is to allow for the development of a quality designed business park. This would be consistent with the current Industrial designation in the Official Community Plan and consistent with City policies pertaining to the provision of employment centres. Another option, as recommended by the Economic Study, would be to accommodate residential uses on this site because a residential population would provide more beneficial long-term support to the Town Centre than would a working population.

The easterly portion of the Highway's site (at the intersection of the two highways) is exposed to a high volume of truck traffic and noise from Highway No. 10 and No. 15. In view of this, it may not be appropriate for any form of multi-family residential. However, this is considered to be a prominent corner with the potential to accommodate an attractive landmark or gateway into the Cloverdale Town Centre. To achieve this, the easterly portion of the property could be considered for commercial uses and could be developed in a comprehensive manner in conjunction

with either the adjacent future residential uses or with a business park component to the west. Possible commercial uses could include a hotel/motel and tourist-related services such as family restaurants and/or fast-food outlets. The Land Use Plan incorporates both land use scenarios as sub-options to provide flexibility.

Overview of the Urban Design Concept (*Section VIII in the attached Plan Document*)

The Plan is focused on injecting new vitality, developing a strong sense of place and establishing an identifiable character for the Town Centre by building on its unique railway and agricultural past. The Plan makes use of the authentic heritage/historical roots of the town site. The main components of the Urban Design Concept are:

- (a) *Pedestrian-Oriented Streets and Corridors*
 - The North-South Axis: “The Station Walk” (176 Street) and “King Street” (176A Street)
 - The East-West Axis: “The Village Walk”

- (b) *Urban Spaces and Activity Nodes*
 - The Heritage Gardens
 - Station Square
 - Turntable Square
 - Hawthorne Square and Paseo
 - Clover Valley Marketplace
 - Clover Valley Junction
 - Clover Place
 - Discovery Square

- (c) *Residential Developments In or Near Town Centre Area*
 - “The West Residential Anchor”
 - “The Melrose Avenue West (57 Avenue) Corridor”
 - “The Village Walk (57A Avenue) Residential Area”

- (d) *Perimeter Routes, Gateways and Landmarks*

The complete Urban Design Concept is presented in Section VIII of the attached Plan Document.

Overview of the Economic Analysis

In November 1999 Coriolis Consulting completed an economic assessment of the Cloverdale business area to assist the City in formulating land use options and strategies. The Coriolis group analysed the Town Centre’s prospects for future commercial and office development, identified strategies that could increase its commercial growth potential, and suggested actions that will help revitalize the existing commercial core by attracting new businesses, increasing sales and generally improving the character of the area.

The findings of the Economic Study are summarized below and were incorporated into the final plans and development strategies for the Cloverdale Town Centre.

- (a) Cloverdale's commercial area is large for a community that is close to a major commercial centre (the Willowbrook Regional Shopping Centre). This is because some of Cloverdale's commercial space pre-dates commercial development in the nearby regional centres and because Cloverdale has a role serving non-resident employees at Cloverdale's industrial areas, tourists and highway travelers.
- (b) The Town Centre's development potential is highly dependent on the overall rate of population growth in the trade area.
- (c) It is estimated that Cloverdale currently has about 61,000 square metres (656,000 square feet) of retail/service floor space and about 17,000 square metres (181,000 square feet) of office floor space.
- (d) Coriolis projects that by 2006, under a low growth scenario, the Cloverdale trade area could support about 20,400 square metres (220,000 square feet) of new retail/service floor space and 6,000 square metres (64,000 square feet) of new office floor space. Under a high growth scenario, the Cloverdale trade area could support about 35,500 square metres (382,000 square feet) of new retail/service floor space and about 10,400 square metres (112,000 square feet) of new office floor space.

The Consultant also suggested several ways in which the Town Centre's commercial development prospects could be maximized. These are outlined in Section VI of the attached Plan document.

Implementation

One of the most important issues arising from the planning process for the Cloverdale Town Centre is that of ensuring that the Plan is effectively and efficiently implemented. There are a number of public and private sector projects along with general street beautification initiatives that together, will allow the Town Centre to move toward increased economic and social vitality. The financial implications of the Plan are substantial. Given the limited capital funding available to the City, if the Plan were to rely exclusively on City-funding for the on-street improvements, substantial improvements to the Town Centre will not be possible for a number of years. However, incremental projects undertaken by the developers in conjunction with redevelopment will allow the new vision to be realized in a measured way over time. The financial implications of the Plan along with some ideas for implementation are summarized below.

- (a) **Street and Circulation Improvements:** The physical aspects of the Cloverdale Town Centre Plan represent a major cost for which the City has not allocated specific funds. Innovative funding arrangements involving the private sector, business associations and community groups will need to be investigated to further the eventual redevelopment of the Town Centre. A ballpark estimate of the total cost excluding

land sales and acquisition, is about \$7 million which includes all of the proposed street and sidewalk improvements, new intersection construction and pedestrian crossings. A breakdown of this total is contained in Table No. 4 in the attached Plan Document.

- (b) **Site Development Projects:** There are five major development/redevelopment projects proposed for the Town Centre at locations shown on Map No. 15 in the attached Plan document (Sites 1 to 5). These projects include both private and public initiatives.
- (c) **Major Contributing Projects:** A number of major projects “in the works” will provide a significant economic boost to the local Town Centre economy, are consistent with the new vision for the Town Centre and will hopefully act as a catalyst for increased investment and interest in the Town Centre in the short term. These major projects are: a) Clover Valley Market Place (Cloverdale Mall redevelopment), b) Hawthorne Square and Clover Valley Junction and the new entrance off of Highway No. 15 (in conjunction with the Mall), and c) the Learning and Discovery Centre at the Fairgrounds (if approved).
- (d) **Improvements Through Private Development Proposals:** As new developments proceed in the Town Centre, some of the street improvements will be undertaken in conjunction with rezoning or development permit approvals.
- (e) **Community Projects:** To contribute to and expedite the new vision for the Cloverdale Town Centre, it will be necessary for many of the urban enhancement projects to be initiated and funded by the community. Projects could be facilitated through the Cloverdale Business Improvement Area Association and other groups, and could include such initiatives as public art and street beautification.
- (f) **Land Acquisition and Exchange:** The Plan for the Town Centre identifies several projects which require the acquisition of certain privately owned lands and the sale, development, and redevelopment of existing City-owned lands within the Town Centre. A discussion of these initiatives along with comments on the financial implications are contained in the attached Plan document (Section IX.F.).

Overall, the land acquisition costs are expected to be lower than the revenues the City will receive from land sales in the Town Centre. Funding required for the purchase of parking areas adjacent to the commercial core should be funded through the community parking program.

It is recommended that Council authorize staff to bring forward a supplementary report on land acquisition issues in support of the Plan, particularly, the acquisition of strategic sites related to important public elements in the Plan.

- (g) **On-going Infrastructure Upgrades:** In conjunction with the City’s normal road and utility upgrade program, certain street improvements may be undertaken in

accordance with the Urban Design Concept as the infrastructure in the Cloverdale Town Centre is repaired or replaced.

- (h) An Implementation Team: Toward ensuring the on-going planning and implementation of the Cloverdale Town Centre, it is recommended that a City Implementation Team be established under the direction of the General Managers of Engineering, Planning & Development, Finance and Parks, Recreation & Culture, and the Economic Development Manager to explore ways by which improvements, beautification and redevelopment in the Cloverdale Town Centre and in Surrey's other four Town Centres can be efficiently managed, promoted and prioritized to enhance their economic sustainability and ability to attract new business while expanding the City's overall commercial tax base.

CONCLUSION

The Cloverdale Town Centre Land Use Plan and Urban Design Concept is a comprehensive planning and design document which establishes land use patterns, vehicular and pedestrian circulation concepts and open space systems. The urban design component of the Plan provides design direction to developers and the general public to achieve the community-wide objectives derived through the planning process. Various implementation measures and suggestions are also contained within the Plan. The entire Plan has been developed with extensive consultation with the public and it is widely supported in the community. It is therefore recommended that Council approve the Plan and authorize staff to proceed with actions to implement the Plan.

(Signed) Murray D. Dinwoodie
General Manager
Planning & Development Department

WW/kms

Attachment - (Cloverdale Town Centre Land Use Plan and Urban Design Concept)

c.c.: - City Manager
- City Clerk

Item No. C013

Cloverdale Town Centre Land Use Plan and Urban
Design Concept
File: 2125-203

It was Moved by Councillor Hunt
Seconded by Councillor Watts
That Council:

1. Approve the attached Cloverdale Town Centre Plan and Urban Design Concept as a means for directing and managing the design, development and revitalization of the Cloverdale Town Centre;
2. Amend the Cloverdale Local Area Plan by including the attached Cloverdale Town Centre Land Use Plan and Urban Design Concept;
3. Authorize staff to process applications to amend the Official Community Plan (Land Use Designation Map) in conjunction with rezoning applications, to implement the Cloverdale Town Centre Plan;
4. Authorize staff to draft amendments to Surrey's Subdivision and Development By-law to identify the road design, tree planting and other applicable standards generally set forth in this report;
5. Direct that staff of the Engineering, Planning & Development, Finance, and Parks, Recreation & Culture Departments, and the Economic Development Manager explore ways by which improvements, beautification and redevelopment in the Cloverdale Town Centre and in Surrey's other four Town Centres can be efficiently managed, promoted and prioritized to enhance their economic sustainability and ability to attract new business while expanding the City's overall commercial tax base;
6. Authorize City staff to continue to meet with and support the Cloverdale Business Improvement Association in its endeavours to promote and attract business and to achieve the revitalization initiatives contained in the Cloverdale Town Centre Plan and Urban Design Concept; and
7. Authorize staff to bring forward a supplementary report regarding land acquisition issues in the Town Centre, particularly with respect to strategic lands required to implement significant components of the Plan.

RES.R00-2646

Carried



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CITY OF PARKS