

FLEETWOOD TOWN CENTRE

LAND USE PLAN and URBAN DESIGN CONCEPT PLAN

DECEMBER, 2000

Planning and
Development Department

FLEETWOOD TOWN CENTRE LAND USE PLAN AND URBAN DESIGN CONCEPT

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FLEETWOOD TOWN CENTRE LAND USE PLAN AND URBAN DESIGN CONCEPT

I. INTRODUCTION

This comprehensive planning and design document provides land development and design policies to guide future development in the Fleetwood Town Centre. It establishes land use and density patterns, vehicular and pedestrian circulation concepts and open space systems. The urban design component of the Plan will provide directions to achieve the community-wide objectives articulated through the planning process. The servicing and infrastructure strategies are presented in a separate engineering document and must be used and implemented in conjunction with the land use and design components of the Plan.

II. BACKGROUND

A. Planning and Population Growth

The current Land Use Plan for the Fleetwood Town Centre was prepared in the late 1970's as part of the larger Fleetwood Local Area Plan, and was subsequently incorporated into the 1986 Official Community Plan. In 1981 the population of Fleetwood was estimated to be 10,100 and was projected to increase to 36,500 by 2001. Fleetwood has grown at a much faster pace than was anticipated. Its population reached 35,800 by 1996 and is estimated to currently be 43,700. It is projected that the population of Fleetwood will be 60,000 by 2021.

The current estimated population in the Town Centre area is about 9,000. This population is expected to double by the time the Town Centre is fully developed.

B. Community Issues

1. Community Concerns

The Fleetwood residents and their Community Association requested the City to review planning issues and directions for the Fleetwood Town Centre. The community expressed a strong desire for a new land use plan that will provide a sense of direction as to how the Fleetwood Town Centre will grow into a pedestrian-oriented vibrant centre, instead of a continuation of the auto-oriented, commercial strip development that historically has characterized the Fraser Highway through the core of the Town Centre.

2. Scattered Commercial Development

Historically, commercial developments have been scattered along Fraser Highway through Fleetwood. This trend did not produce concentrations of commercial development that could transform into an identifiable community centre to successfully serve the growing population of Fleetwood. The commercial development was mainly auto-oriented and not effectively connected to the nearby residential areas. In general, the Fleetwood residents felt disassociated from commercial services in the area and expressed a need for a community focus.

3. Competing Nodes

The small-scale commercial developments currently scattered around the intersection of Fraser Highway and 160 Street evolved in the past to serve the semi-rural community of Fleetwood (when it was predominantly single-family homes on large holding lots). A more substantial commercial node exists at 152 Street and the Fraser Highway. This commercial node was considered to be the centre of the Fleetwood community until the 1986 Official Community Plan permitted a major eastward expansion of urban land uses within Fleetwood. As a result of this expansion, the intersection at 84 Avenue and 160 Street was designated as the Town Centre of Fleetwood due to its more central location within the entire Fleetwood area. The challenge today is how to build a strong and viable community around this “centre” in consideration of the existing and future demand for new commercial services as Fleetwood continues to grow.

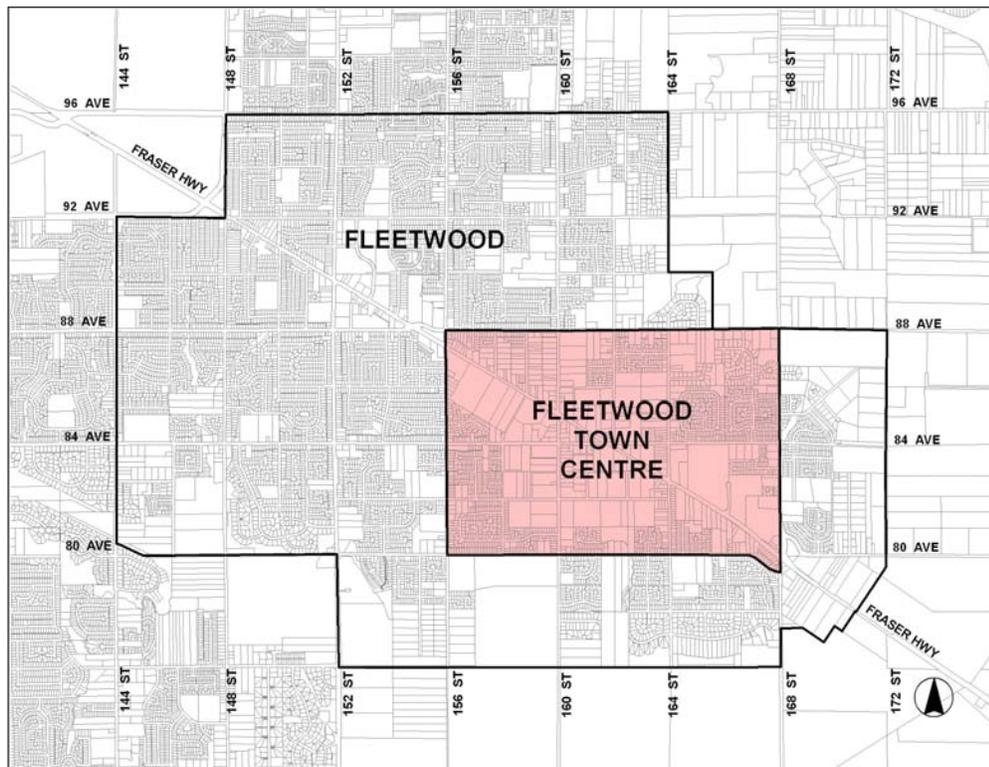
III. THE PLAN AREA

The Fleetwood Town Centre planning area contains about 350 hectares (864 acres) and is focused on the designated centre of Fleetwood at the intersection of the Fraser Highway and 160 Street and 84 Avenue. The planning area is bounded on the north by 88 Avenue, the south by 80 Avenue, the west by 156 Street and the east by 168 Street. Map No. 1 illustrates the boundaries of the planning area within the context of the Fleetwood community.

IV. OBJECTIVES OF THE STUDY

The objectives of the Fleetwood Town Centre planning study were:

1. To update the Fleetwood Town Centre plan due to the changes that have occurred in Fleetwood since the previous plan for the area was approved;
2. To review the adequacy of land designated for Town Centre uses in the long term and the nature and form of development desired by the residents and property owners;
3. To prepare a servicing and financial strategy to support the Land Use Plan; and
4. To develop an urban design concept to create cohesion, a sense of community identity and a unique character for the Town Centre area.



Map 1. Planning Area Boundary

V. THE PLANNING PROCESS

A. Overview

The planning process commenced in late 1997 with the engagement of a consultant (Plancon Consultants Inc.) to review the current land uses, initiate the public consultation process and develop a generalized land use plan for the Fleetwood Town Centre. This work, which was concluded in March 1998, provided the basis for the initial (Stage 1) Land Use Plan.

The review of the Fleetwood Town Centre has two components:

1. Stage 1: A physical plan for detailed land uses (approved in principle by City Council in April 1999); and
2. Stage 2: An urban design concept in support of the Land Use Plan, and a servicing and financial strategy for the provision of services and amenities.

The Corporate Report and Stage 1 Land Use Plan are on record at Surrey City Hall. This final report (Stage 2) consolidates the land use policies and presents the urban design concept to guide future development within the Fleetwood Town Centre.

B. Public Consultation and Planning Activities

The Planning & Development Department established an in-house Steering Committee and undertook an extensive process of public consultation during the preparation of the Fleetwood Town Centre Plan. Consultation with the community included three Public Information Meetings, open houses, informal and formal surveys of the residents and on-going discussions with residents, property owners, merchants, the Fleetwood Community Association and the development industry. Table No. 1 (next page) summarizes the various planning and consultation initiatives undertaken during the planning process.

Table No. 1 – The Planning Process

| Date | Activity |
|------------------------|--|
| March 1997 | Plancon Consultants retained to review current land uses, initiate public consultation process and develop generalized Land Use Plan. |
| April 1997 | City Steering Committee formed including staff from Planning & Development, Parks, Recreation & Culture, and Engineering Departments. |
| August 1997 | <p>Public Information Meeting held at Fleetwood Community Centre: approximately 200 people in attendance.</p> <p>Two concepts of the future Town Centre were presented:</p> <p style="padding-left: 40px;">Concept 1 – Expanded Town Centre focused upon commercial uses along the Fraser Highway</p> <p style="padding-left: 40px;">Concept 2 – Expanded Town Centre focused upon commercial uses along 84 Avenue and 160 Street north of the Highway and containment of highway commercial uses along the Fraser Highway.</p> <p>500 questionnaires distributed at the meeting and 125 were returned.</p> <p>Responses and other consultations indicated a preference for Concept 2.</p> |
| August 1997 | Plancon Consultants prepare generalized Land Use Plan, in consultation with City Steering Committee, reflecting community responses to the concepts presented at the public meeting. Generalized Land Use Plan indicates expansion of the Town Centre north of the Fraser Highway along 160 Street and containment of highway commercial uses along the Fraser Highway. |
| September 1997 | <p>Public Information Meeting held at Fleetwood Community Centre: approximately 150 people in attendance.</p> <p>Approximately 50 responses received as well as numerous comments and concerns expressed during and after the meeting.</p> <p>Overall community support for the generalized plan.</p> |
| October – March 1998 | Plancon Consultants refines generalized Land Use Plan in response to community concerns raised at public meeting. Plancon Consultants submit final report in March 1998. |
| April – May 1998 | In consultation with the Steering Committee, residents and property owners, the Planning & Development Department prepares detailed Land Use Plan based upon the plan prepared by Plancon Consultants. |
| June – September 1998 | Proposed detailed Land Use Plan displayed at the Fleetwood Community Centre for a number of days, including during the 75th Anniversary Celebrations of the Fleetwood Community Association. |
| June 1998 – March 1999 | <p>Proposed Land Use Plan on display at City Hall.</p> <p>Refinement of Land Use Plan in response to discussions with other government agencies, residents and property owners and development industry representatives.</p> |
| April 1999 | Land Use Plan (Stage 1) approved in principle by City Council and seven recommendations approved (see Section B). |
| May 1999 – July 2000 | Planning staff commenced Stage 2 including a review of the amenity requirements, urban design guidelines and pedestrian and bicycle network plan. Engineering staff prepared a comprehensive servicing and financial plan to demonstrate the funding requirements and provisions for infrastructure. |
| July 2000 | City staff presented the Stage 2 components of the Plan to the Community Association president and discussed a strategy for public review of the final Plan. |
| September 2000 | <p>Final Land Use Plan, Servicing and Financial Strategy and Urban Design Concept presented at a Public Meeting held at the Fleetwood Community Association: approximately 200 people in attendance.</p> <p>Responses and other consultations indicated general community support for the Plan; some refinements made in response to localized concerns.</p> |
| November 2000 | City Council's consideration of the complete Fleetwood Town Centre Land Use Plan, Urban Design Concept and Servicing Strategy. |

VI. FLEETWOOD TOWN CENTRE LAND USE PLAN AND POLICY DIRECTIONS

A. General Planning Principles

Throughout the community consultation process, Fleetwood property owners provided many comments, concerns and ideas about their vision for the Town Centre. These have been developed into a number of principles that guided the preparation of this Land Use Plan (and the Urban Design Concept). These principles are:

1. The character of the established residential neighbourhoods should be protected;
2. A mix of commercial and residential uses should be encouraged within the Town Centre;
3. Highway commercial uses should be contained along the Fraser Highway;
4. Community commercial uses should be developed along 84 Avenue and along 160 Street (including north of the Fraser Highway);
5. Strong pedestrian connections should be provided between public facilities, commercial activities and residential development, on both sides of the Fraser Highway;
6. A safe pedestrian and bicycle environment should be created in the Town Centre;
7. Public plazas and wide sidewalks should be developed to support community commercial development and to provide informal community gathering spaces;
8. Sufficient population densities should be provided to support public transit and to create a vibrant Town Centre;
9. A broad range of housing types (single family dwellings, townhouses and apartments) should be provided to meet the future housing needs of all residents; and
10. The road network should be designed and completed to improve vehicular circulation within the Town Centre.

B. The Land Use Plan

The Fleetwood Town Centre Land Use Plan is shown on Map No. 2. This Plan identifies the distribution of the various land uses, densities, road network and open space linkages. The Plan will be used to guide development decisions for new development in the Town Centre and is supplemented by general policies and urban design guidelines contained

in this document. The main objectives of the Land Use Plan (and the supporting Urban Design Guidelines) are:

1. To reinforce the role of the Fleetwood Town Centre in the community;
2. To enhance the Town Centre and enable it to be a successful, attractive commercial area;
3. To provide direction for the physical development of the Town Centre and identify site development opportunities and improvements to the public environment;
4. To create a unique character for the Town Centre; and
5. To maintain and enhance the character of the Town Centre by establishing design guidelines.

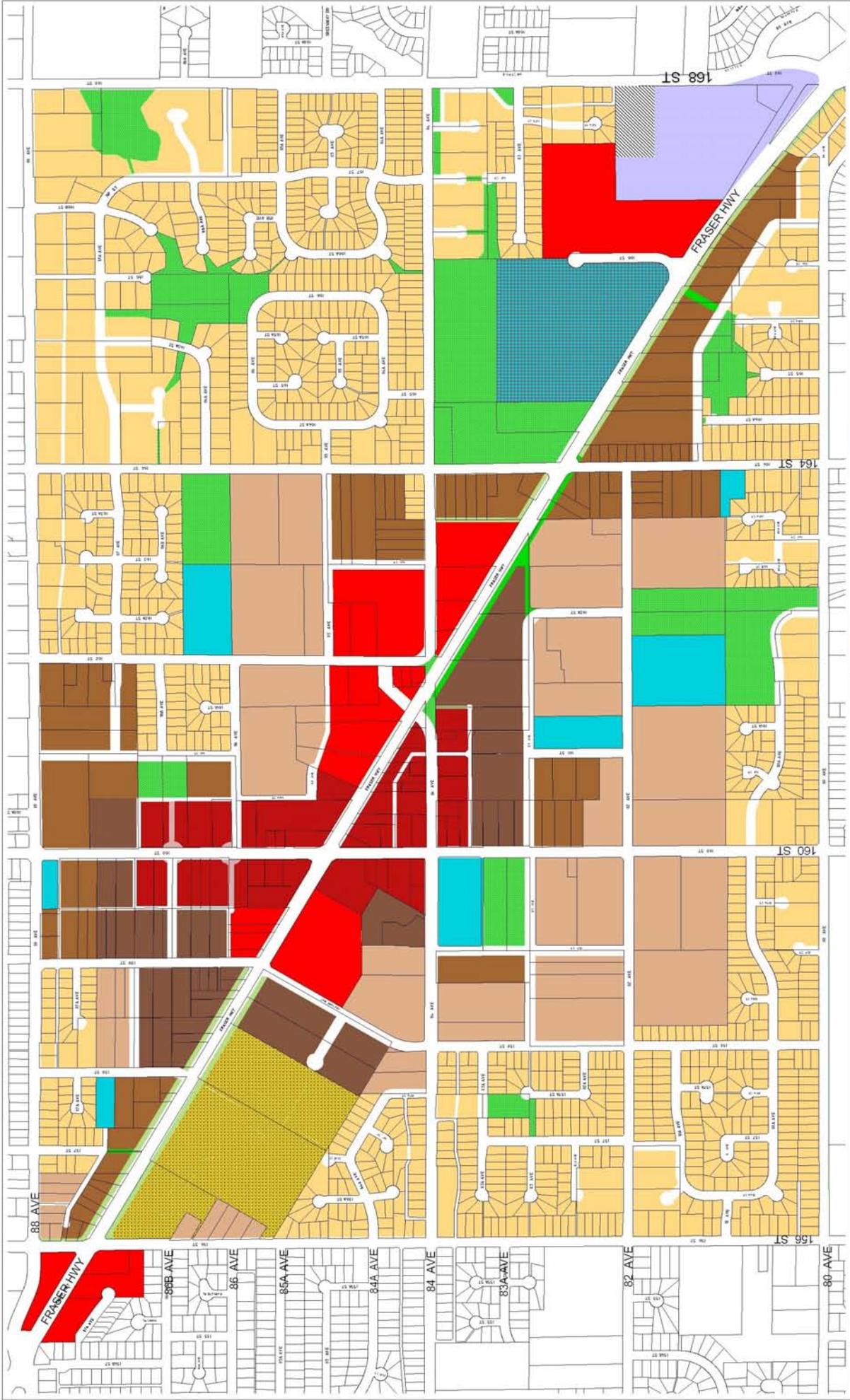
C. Commercial Development in the Town Centre

1. General

The Land Use Plan accommodates the following types of commercial land uses within the Town Centre:

- *Community Commercial*: higher order pedestrian-oriented commercial uses and mixed-use commercial/multiple residential developments (this does not include residential developments that do not have a commercial component, or auto-oriented commercial uses such as car sales or gas stations);
- *Highway Commercial*: auto-oriented commercial uses (this does not include multiple residential or commercial multiple residential mixed-use developments); and
- *Local and Neighbourhood Commercial Uses and Child Care Facilities*: small neighbourhood convenience services can be considered throughout Fleetwood and will be evaluated on a case-by-case basis.

To respond to the desire of Fleetwood residents for a compact, pedestrian-oriented community, it is necessary to redirect the focus of the Fleetwood Town Centre away from highway commercial uses by containing the spread of automobile-oriented commercial activities along the Fraser Highway and by expanding the opportunity for community commercial uses to locate within walking distance of residential areas. A commercial core will be shaped to lend a synergy to existing commercial development in Fleetwood, thereby attracting businesses and customers and creating a vibrant identifiable commercial centre.



MAP No. 2 Land Use Plan

Nov. 1, 2000

SCALE: 1:2500

SC#3

FLEETWOOD TOWN CENTRE

Nov. 1, 2000

SCALE: 1:2500

SC#3

Community Commercial (131,466 m²)

Highway Commercial (309,346 m²)

Apartment & High Density Townhouse (189,332 m²)

Medium Density Townhouses (303,554 m²)

Low Density Townhouses (510,153 m²)

Single Family (124,856 m²)

Manufactured Homes (120,502 m²)

Industrial (66,417 m²)

Institutional (88,000 m²)

Parks & Linear Corridors (255,156 m²)

Institutional / Commercial (84,591 m²)

Multisuse Corridor

Buffer Within Private Land

N

W

E

S

NOTE: This plan is incorporated in nature and is intended to reflect a general pattern of land uses and street design.



An easily accessible commercial core is proposed to be located along 160 Street from 84 Avenue to 87 Avenue which will serve as a main commercial street for the entire Fleetwood community. A commercial/community services node is proposed south of the Fraser Highway as an anchor at the intersection of 160 Street and 84 Avenue (in conjunction with the Fleetwood Library and Community Centre and eastward toward the Fraser Highway). Pedestrian-oriented commercial uses are proposed along 160 Street: the northern area (north of Fraser Highway) is expected to accommodate shopping and retail, and the southern area is intended to accommodate community services such as medical offices and civic facilities.

2. Commercial Policy Directives

In view of the above, the following policy directives will guide the location and general characteristics of commercial development within the Fleetwood Town Centre:

- a) Highway commercial activities will be limited to those properties along the Fraser Highway that are currently zoned and/or designated for such uses in the Official Community Plan.
- b) The few properties along the Fraser Highway that are currently zoned for highway commercial uses but located outside of the commercial core are to be developed with higher density residential uses.
- c) While the centrally located triangle of properties formed by 84 Avenue, 160 Street and Fraser Highway are currently dominated by highway commercial activities, this area will develop toward community commercial uses.
- d) Pedestrian-oriented commercial uses will be developed along 160 Street from 84 Avenue to 87 Avenue. This commercial street will constitute the community focus and commercial core for the residential areas on the south and north sides of the Fraser Highway.
- e) The intersection of 160 Street and 84 Avenue will constitute the southerly terminus of the commercial street and may be developed as a contained southerly commercial/community services node extending eastward to the Fraser Highway.

D. Residential Development in the Town Centre

1. General

The Fleetwood Town Centre Land Use Plan accommodates five residential land use designations as follows:

- a) *Apartment and Medium Density Townhouses*: multiple residential developments with maximum densities of 70 units per acre (4-storeys);
- b) *Medium Density Townhouses*: multiple residential developments with maximum densities of 30 units per acre;
- c) *Low Density Townhouses*: multiple residential developments with maximum densities of 15 units per acre;
- d) *Single Family Residential*: single family developments with varying lots sizes and densities ranging from 6 to 12 units per acre; and
- e) *Manufactured Homes*: manufactured or “mobile home” developments (existing).

The Land Use Plan attempts to protect existing residential areas by providing appropriate transitions between residential and commercial and between different residential densities. It also provides for interconnectivity between the residential areas and the commercial core area and recommends a general building height restriction to foster a more pedestrian scale environment. To support viable commercial activities in the core commercial area, higher density residential development is located in close proximity to the commercial streets.

2. Residential Policy Directives

The following policy directives will guide the location and general characteristics of residential development within the Fleetwood Town Centre:

- a) The existing single-family residential areas and townhouse developments will be retained and protected.
- b) On the south side of the Fraser Highway, apartments and medium density townhouse uses will only be located adjacent to the existing mobile home park and

- existing apartments, and as a transition between commercial uses along the Fraser Highway and lower density residential areas.
- c) Undeveloped properties located between existing single-family neighbourhoods and low density townhouse projects may be developed with lower density townhouse uses.
 - d) Medium density townhouses (three storeys) may be developed as transitional uses between single family or low density townhouse areas and institutional uses, commercial uses or apartments.
 - e) Apartments and medium density townhouse uses will be located adjacent to the community commercial uses on the north side of the Fraser Highway.
 - f) To permit flexibility to respond to the marketplace, some higher density sites, as identified on the Land Use Plan, may be developed either as apartments or townhouses.
 - g) The integrity of the established residential areas in the Fleetwood Town Centre will be respected by requiring adjacent new developments to have similar building massing along the interfacing street or property line. Some of these areas requiring attention to the interfacing are identified on the Land Use Plan.
 - h) To increase overall residential densities in the Town Centre area, single family residential uses at the edges of the Town Centre core or at the interface with townhouse developments should consider narrow lot single-family units with “coach houses” and duplexes.
 - i) To achieve a “village-like” environment in the Town Centre, the height of buildings in the Fleetwood Town Centre will be a maximum of 4 storeys (15 metres/50 feet).

E. Industrial

1. General

There are several properties within the Town Centre in the vicinity of 162 Street and 84 Avenue that are designated Industrial in the Official Community Plan and which have been developed with highway commercial uses (which is a permitted land use in the Industrial OCP designation). These properties are identified in the Land Use Plan as “highway commercial” and are expected to remain as such.

In the eastern area of the Town Centre, the BC Gas site is designated Industrial in the Official Community Plan. It contains an office building and some storage buildings. The Town Centre Land Use Plan designates this site for industrial uses which may include business park, light impact industrial or highway commercial uses.

2. Industrial Policy Directives

- a) Lands located at 162 Street and 84 Avenue which are designated Industrial in the Official Community Plan will remain as such and will accommodate highway commercial uses and offices.
- b) Lands located at the north-west corner of the Fraser Highway and 168 Street which are designated Industrial may accommodate business park, light impact industrial or highway commercial uses.

F. Institutional Uses and Community Facilities

Existing institutional uses in the Town Centre such as private schools, places of worship, care facilities and public buildings (such as schools, community centres and fire halls) are recognized in the Land Use Plan under the Institutional land use designation. The Surrey Sports and Leisure Complex site is recognized as a destination in the community and is accommodated through an institutional/commercial designation. The Fleetwood Library and Community Centre/Park area is also recognized as a destination and “civic” centre of Fleetwood. The existing schools in the Fleetwood Town Centre are able to accommodate the projected school enrolment resulting from new residential development in the area. New institutional uses in the Fleetwood Town Centre will be permitted in any of the land use designations.

G. Parks and Open Space

There are three neighbourhood school/park sites within the Fleetwood Town Centre along with a sizable neighbourhood park in the north-east residential area, a large park area adjacent to the Leisure Centre on the Fraser Highway, and other small neighbourhood parks, open spaces and greenways. There will also be several urban plazas along the shopping street which are further described in Section VII (Urban Design Concept). The main parks are connected to the residential areas and to destinations in the community. Map No. 3 identifies where the parks and other open spaces are located.

H. Pedestrian and Bicycle Circulation

The pedestrian and bicycle network in the Fleetwood Town Centre is based upon a series of inter-connected routes (on and off-street) that provide easy and safe access to the commercial core from residential areas and convenient connections to all of the destination points in the community. The design characteristics of the network bring continuity and delineation to the network and are illustrated in Section VIII of this document. The pedestrian and bicycle circulation network is shown on Map No. 3, and includes:

1. Core Pedestrian Spines

These establish strong pedestrian priority and community image and are characterized by wide sidewalks, special pavement and lighting, trees in grates and design co-ordinated street furniture.

2. Town Centre Pedestrian Routes

These are direct access routes to the commercial core, are organized as a loop to facilitate movement in and around the retail core of the Town Centre and are characterized by treed boulevards, pavement accents and special lighting.

3. Secondary Pedestrian Routes

These are feeder routes from surrounding areas and link to the Town Centre Pedestrian Routes.

4. Neighbourhood Pedestrian and Bicycle Routes

These are a combination of on and off-street routes that provide access to park areas and to the main pedestrian/bicycle routes.

I. Land Use Statistics

Table No. 2 presents the estimated population, housing types and commercial/industrial floor space for the Fleetwood Town Centre for 1998 and at full build out.

Table No. 2 – Development Projections Based on the Land Use Plan

| | 1998 (Estimated) | Estimated Density | Land Area (acres) | Dwelling Units at Saturation |
|---------------------------------------|-----------------------------|------------------------------|----------------------------------|---|
| Population | 9,000 | - | - | 19,957 ¹ |
| Number of Dwelling Units | | | | |
| Single Family Dwellings | 1,415 | 7 upa | 307 | 2,148 |
| Medium Density Townhouses | 500 | 22 upa | 74.5 | 1,639 |
| Low Density Townhouses | 500 | 12 upa | 125.6 | 1,507 |
| Apartments | 240 | 45 upa | 46.6 | 2,097 |
| Manufactured Homes | 255 | Existing | 29.8 | 255 |
| Total Number of Dwelling Units | 2,905 | - | - | 7,646 |

| | 1998 (Estimated) | Estimated Density | Land Area (acres) | Floor Space at Saturation |
|--|--|------------------------------|----------------------------------|---|
| Community Commercial | - | 0.3 FAR | 32.2 | 39,094 m ² (420,818 ft ²) |
| Highway Commercial | - | 0.4 FAR | 51.7 | 83,692 m ² (900,883 ft ²) |
| Industrial | - | .5 FAR | 16.9 | 34,197 m ² (368,105 ft ²) |
| Total Commercial, Industrial & Office Floor Space | 77,500 m² (834,230 ft²) | - | - | 156,983 m² (1,690,000 ft²) |

| | | | | |
|---------------|---|--------|-----------------------|---|
| Institutional | - | .5 FAR | 21.7 | 43,910 m ² (472,659 ft ²) |
| Park Space | - | - | 63.8 ha. (158 ac.) | - |

¹ This population is an estimate and is based on 3 persons per household (pph) for single family, 2.8 pph for townhouses and 2 pph for apartments and manufactured homes.

VII. PLAN IMPLEMENTATION

A. Official Community Plan Land Use Designations

Amendments to the current land use designations in the Official Community Plan (OCP) are required to implement the Fleetwood Town Centre Land Use Plan. The existing and proposed OCP land use designations are shown on Map No. 10. It is recommended that the amendments to the OCP be implemented immediately after the Fleetwood Town Centre Plan is approved by City Council.

B. Amenity Contributions

To address the amenity needs associated with new growth in the Fleetwood Town Centre, new development will be required to make a monetary contribution toward the provision of new police and fire protection services, library materials and toward the development of new parks and open space.

The monetary contributions toward police, fire and library materials will offset the capital costs of providing these services to the new development and are applied on a standardized basis in all of Surrey's new urban areas. The monetary contributions toward park and open space are based upon an estimate of the capital costs of these improvements proportioned to the growth expected in the Fleetwood Town Centre. The cost is divided among the anticipated number of dwelling units to ensure an equitable contribution arrangement.

The estimated cost of developing park-related amenities is approximately \$3,000,000. The park-related amenity contribution will go toward:

- the development of artificial turf fields (the estimated total cost is \$3 million of which half of these costs will be borne by the larger Fleetwood community);
- future expansion of the Fleetwood Recreation Centre and park site;
- general park development;
- bridge and pathway construction;
- a Youth Park for the area; and
- a memorial for Thomas Fleetwood.

As it is projected that there will be about 4,500 new dwelling units in the Fleetwood Town Centre, the per unit amenity charge will be \$1,055.88 per unit. The contributions are payable at the subdivision/rezoning stage for

single family subdivisions and at the building permit stage for multiple residential and non-residential development.

The applicable amenity contributions (per unit) and the estimated revenue the City can expect to receive from new residential development are shown in Table No. 3.

Table No. 3 – Amenity Contributions

| | Residential (per dwelling unit) | Non-Residential (per acre)² | Anticipated Revenue³ |
|-------------------|--|---|--|
| Park Development | \$666.666 | - | \$3,000,000 |
| Library Materials | \$115.68 | - | \$520,560 |
| Fire Protection | \$222.11 | \$888.46 | \$999,495 |
| Police Protection | \$51.42 | \$205.64 | \$231,390 |
| TOTAL | \$1,055.88 | \$1,094.10 | \$4,751,445 |

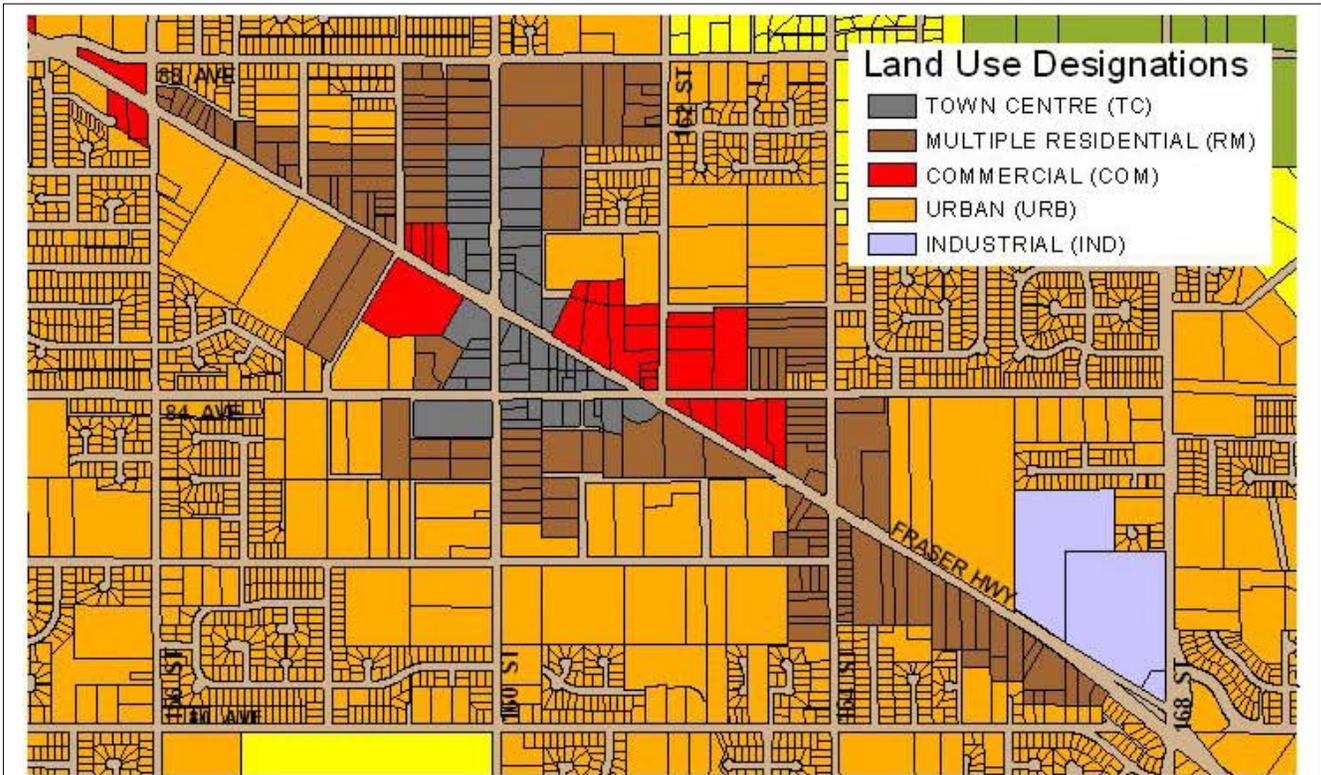
C. Street Improvements and On-Going Infrastructure Upgrades

Most of the street improvements and beautification schemes proposed in the Urban Design Concept apply to 160 Street and to the Fraser Highway. Both 160 Street and the Fraser Highway are classified as arterial roads and their widening/construction will be funded through DCCs (the Fraser Highway may be partially funded by Translink). The 160 Street cross-section through the Town Centre includes wide sidewalks, street trees in grates and other features which will be incorporated into the design of 160 Street when it is upgraded. It is also proposed that the character of the Fraser Highway be changed as it crosses through the Town Centre. This too will be incorporated into the design and construction when Fraser Highway is ultimately widened. Other improvements (i.e., a portion of the small plaza and pedestrian streets, linear pathways and connections) will occur as commercial and/or residential development proceeds in the Town Centre.

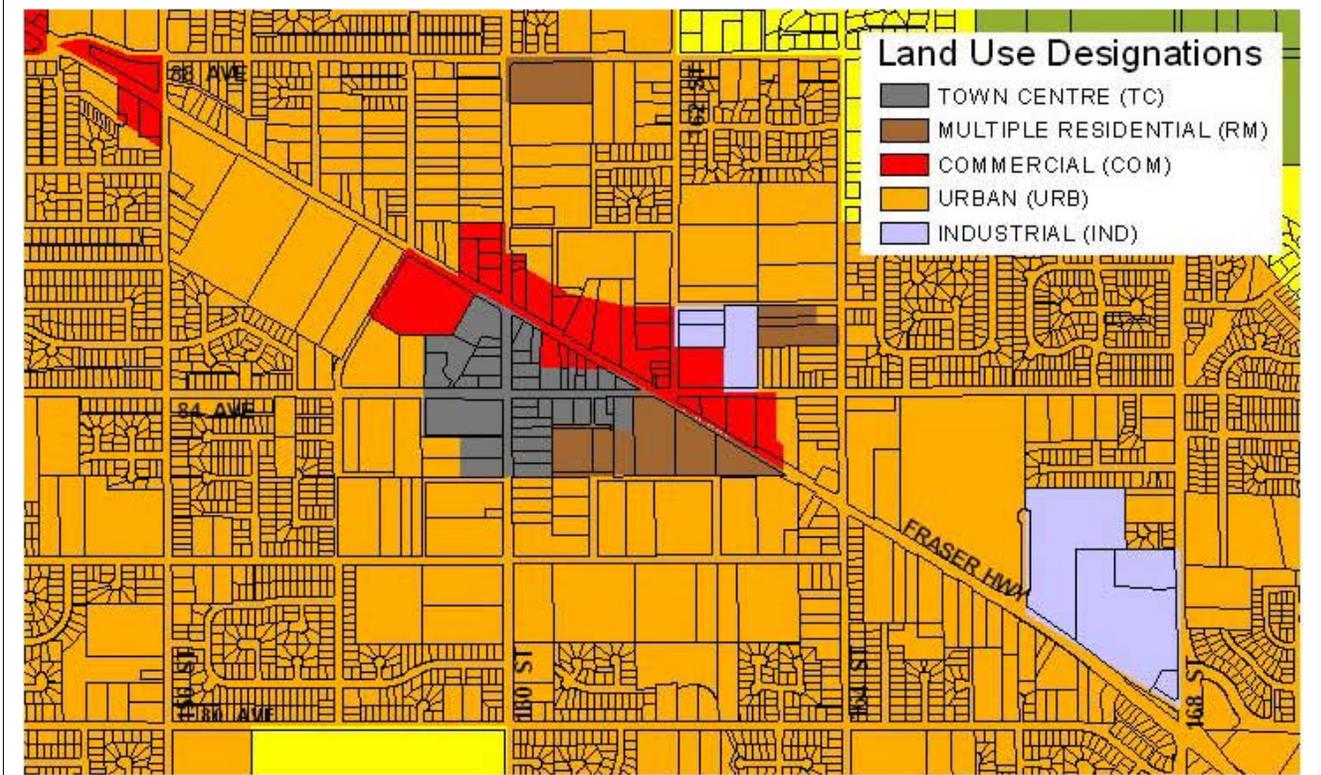
In conjunction with the City’s normal road and utility maintenance and upgrade program, certain street improvements may be undertaken in accordance with the Urban Design Concept as the infrastructure in the Fleetwood Town Centre is repaired or replaced.

² Contributions from non-residential development are based upon an equivalency factor of 1 hectare (2.47 acres) of land being equivalent to 15 residential units.

³ Revenue from residential development.



Fleetwood: Proposed OCP Land Use Designations



Fleetwood: Existing OCP Land Use Designations

Map No. 4 – Existing and Proposed OCP Designations

FLEETWOOD TOWN CENTRE URBAN DESIGN CONCEPT

VIII. FLEETWOOD TOWN CENTRE URBAN DESIGN CONCEPT

A. The Fleetwood Town Centre Urban Structure

1. The proposed urban structure for the Town Centre recognizes the need to define a strong node that serves the community and that is accessible to local resident on both sides of the Fraser Highway by car, bicycle and on foot.
2. In achieving this main objective, the Town Centre Plan also recognizes the presence of highway commercial uses along the Fraser Highway, and the community services node defined by the old Fleetwood Community Hall, the Library and the Senior's/Recreation Centre at 160 Street and 84 Avenue.
3. The Fleetwood Town Centre Plan builds on the existing land use nodes and establishes 160 Street as the north-south retail/commercial spine that serves and links the residential areas on both sides of the highway.

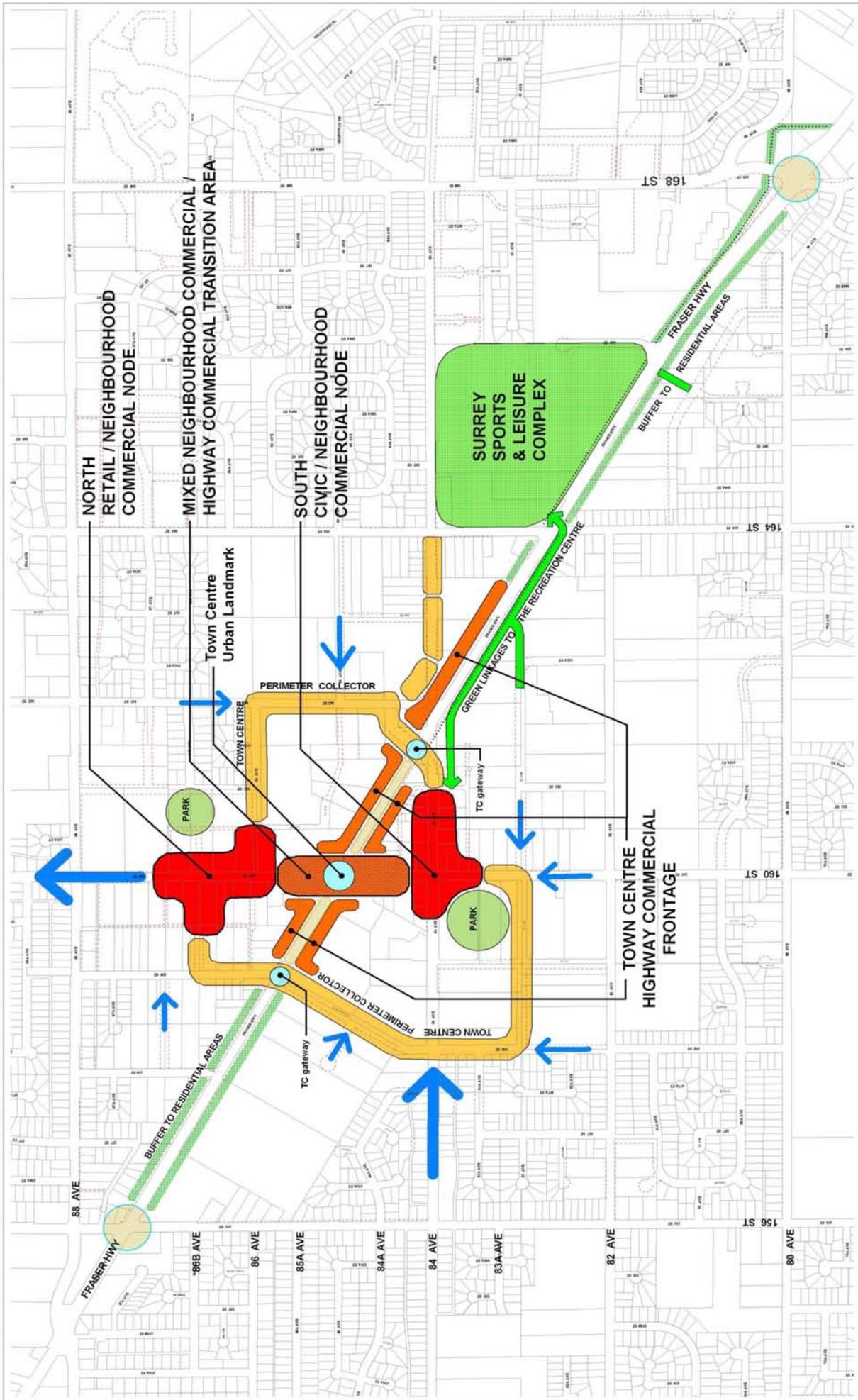
B. Overview of the Urban Design Concept

The Urban Design Concept focuses on highlighting the basic structure of the Town Centre core and provides direction for the comprehensive and coordinated physical development of the Town Centre while also defining and reinforcing its unique, identifiable character.

The vision for the Fleetwood Town Centre is based on the following components of the core area and its identifiable nodes:

1. Two community commercial nodes (north and south of the Fraser Highway);
2. A transition commercial area along the Town Centre Main Street (160 Street);
3. A highway commercial frontage;
4. The Surrey Sports and Leisure Complex; and
5. The Multiple Family Residential areas surrounding the central core.

Map No. 5 provides a visual overview of the components of the Urban Design Concept for the Fleetwood Town Centre.



MAP No. 5 Urban Design Concept

FLEETWOOD TOWN CENTRE

NOTE: This plan is conceptual in nature and is only intended to reflect a general pattern of land uses and street structure.

Scale: 1:1000

Nov. 2, 2000



C. The Town Centre Nodes

The role and character of the core components of the Town Centre are as follows:

1.a. The North Retail Commercial Node

This commercial area is proposed along 160 Street north of the Fraser Highway in an area that is presently undeveloped. It will be developed with a strong pedestrian oriented urban character with wide sidewalks and continuous building frontage. Local retail activities and mixed-use commercial-residential developments are appropriate in this area.

Important components of this node are the two short east-west pedestrian-only commercial streets proposed at 85 and 86 Avenues. These commercial streets will extend the commercial frontage available in the area and will provide easy pedestrian access to the main commercial street from the residential areas located to the east and west of 160 Street. These pedestrian streets will also facilitate the completion of a grid street system which facilitates convenient east-west pedestrian circulation in the area and direct access to the commercial street.

1.b. The South Civic/Commercial Node

This node is focused on 84 Avenue and the Library/Recreation Centre at the 160 Street intersection. This node combines office/retail uses along both sides of the street and community service uses focused on the existing community facilities centered around the Library. Office/retail uses with commercial frontages are proposed to extend eastward to the intersection with Fraser Highway.

2. The Mixed Commercial/Highway Commercial Link

This is a transition area that extends across the Fraser Highway, along 160 Street, between the two commercial nodes referenced above. It includes a combination of existing highway commercial and retail commercial types of development that have parking areas rather than building facades toward the street.

3. The Town Centre Highway Commercial Frontage

This is a strong existing vehicle-oriented edge along both sides of the Fraser Highway between Venture Way and 162 Street. Although the present land uses will likely continue to exist, it is intended that the quality of the built environment be modified to relate to the character of the Town Centre. Reducing the extent of parking areas exposed to views from the Fraser Highway and locating buildings

closer to the Highway will permit a better definition of the highway edges and a more urban character for these commercial land uses.

4. The Surrey Sports and Leisure Complex

The Surrey Sports and Leisure Complex is incorporated as a component of the Fleetwood Town Centre by a short multi-use corridor link (See Map No. 5) that runs along the south side of the Fraser Highway between the 84 Avenue intersection with Fraser Highway and 162 Street. This Complex will be a strong anchor on the east end of the Town Centre core. The Complex is also accessible via 84 Avenue at 162 Street.

5. The Town Centre Core Multiple Family Residential Areas

These more compact residential areas in close proximity to the Town Centre's local commercial spine will help to reinforce the pedestrian-oriented and urban character of the area. Mixed-use developments which combine retail/commercial and residential uses might also be considered for this area for they would help to provide 24-hour activity on the Town Centre's main commercial street (160 Street).

D. The Town Centre Core Vehicular/Pedestrian Network

The Urban Design Concept identifies special treatments for the streets and pedestrian corridors that provide access to the Town Centre core from the east and west, and for the north and south linkages across the Fraser Highway (See Map No. 6).

Primary and secondary streets that define the basic grid of the Town Centre core provide direct linkages to and from the surrounding residential areas. Special characteristics of the streetscape on these streets and the extensions into the single family residential areas help to identify the main access routes to the Town Centre core and the linkages between community activity nodes.

The most important components of the vehicular/pedestrian network in the Town Centre core are the main Retail/Commercial Street (160 Street) and the 84 Avenue commercial street. Another important vehicular corridor is the Fraser Highway, as it crosses through the Town Centre area.

The design and characteristics of these main components of the vehicular/pedestrian network are described below.

1. The Main Retail/Commercial Street (160 Street)

The stretch of 160 Street between 88 Avenue and 84 Avenue will serve as the commercial/retail core of the Fleetwood Town Centre. The main urban design

objective is to create a pedestrian-friendly street where residents can interact with each other without excessive interference from automobile traffic. Further, the design-coordinated streetscape, street furniture, landscaping and building frontage will help to foster a distinct Town Centre character. The dominant features of the proposed design for the main commercial street of the Fleetwood Town Centre are illustrated on Sketch 1 and described below:

The following design features are proposed for 160 Street:

- wide sidewalks on both sides of 160 Street which are lined with trees along the street edge to separate vehicles and pedestrians;
- special paving pattern and materials to give the Town Centre a distinct character; design-coordinated street furniture including benches, garbage receptacles and others will reinforce this unique character;
- continuous commercial/retail storefronts to define the edge of the street;
- all commercial/retail units will have direct access to the sidewalks; additional building frontage setback at specific locations may provide extra space for outdoor pedestrian activity and define store entrances;
- potential for a dedicated on-street parking lane on both sides of the street; small clusters of parking separated by landscaped pockets (curb extensions);
- landscaped pockets (curb extensions) planted with trees and shrubs may also accommodate street furniture (i.e., benches);
- a central median to reduce the visual width of the street; shrubs and deciduous high canopy trees provide greenery while maintaining the visual connection between both sidewalks; and
- extensions of 86 Avenue to the east and of 87 Avenue to the west are to be treated in a manner similar as 160 Street, with a dedicated on-street parking lane on each side.

Urban plazas established at the intersections of the two east-west streets that intersect 160 Street (see Sketch 1). There is a north plaza and a south plaza, each of which is approached by a pedestrian-only street. The North Plaza is located at the intersection of 160 Street and 87 Avenue and the South Plaza is located at the intersection of 160 Street and 86 Avenue. Respectively, these plazas anchor each one of the two pedestrian-only commercial streets proposed in the plan.

The proposed circular configuration of the plaza on the west end of the northern pedestrian-only commercial street is intended to provide a reference point on the main street, concentrate the retail activities and provide for social interaction. Features of this urban plaza are:

- special paving patterns and materials that reinforce the plaza as a special urban space;
- special alignment of streetlights and provision of community landmarks;

- warm-colored paving materials used to identify the pedestrian route across 160 Street and to signal the priority of pedestrians over vehicles at the intersection; and
- corner cuts on buildings on the west side of the street to further open up the intersection and mirror the buildings across the street.

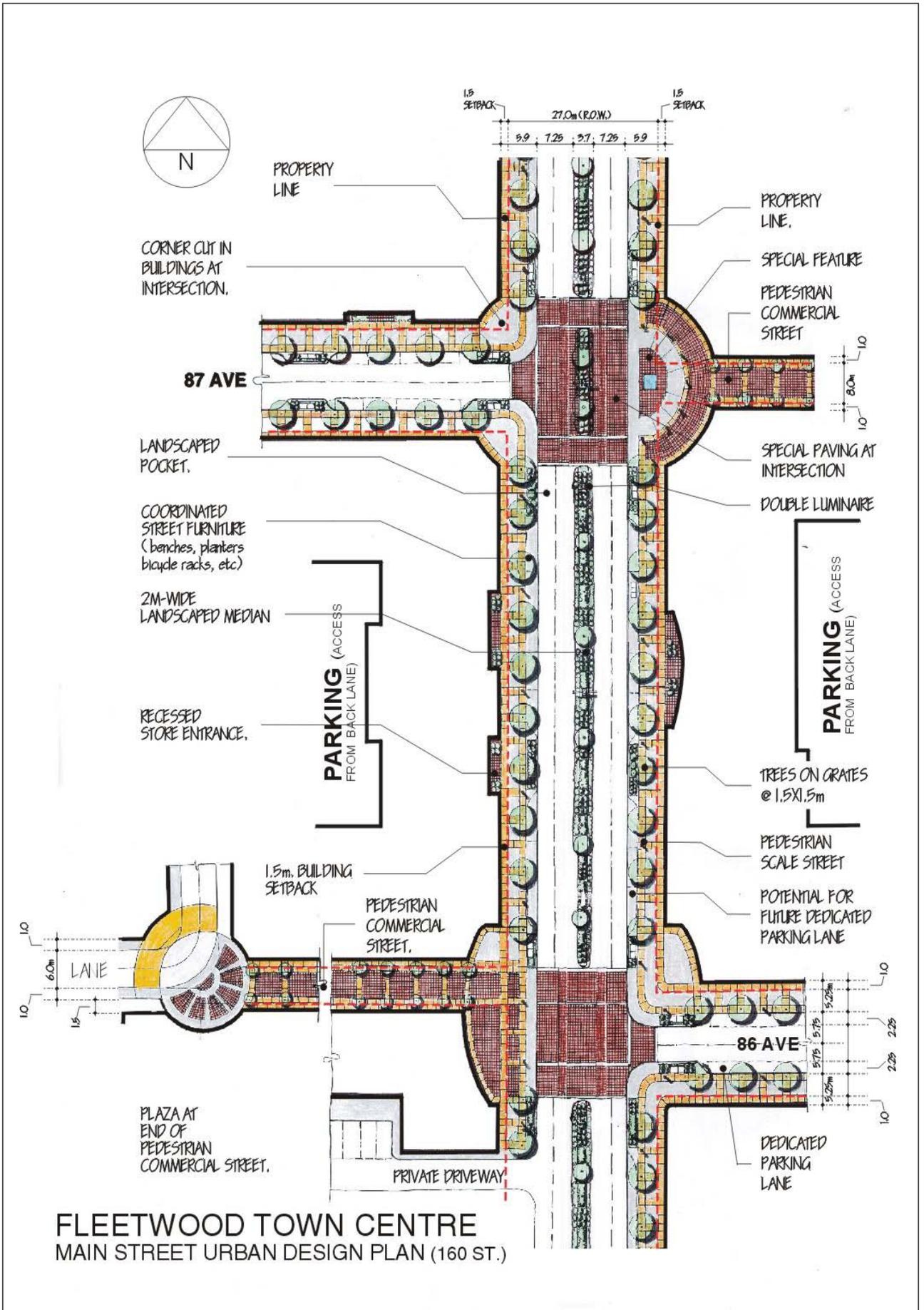
The South Plaza is located at the west end of 86 Avenue. The asymmetrical configuration of the plaza is intended to reinforce visual continuity and pedestrian movement from 86 Avenue to the proposed pedestrian-only commercial street to the west of 160 Street. Features of this urban space are similar to the North Plaza.

2. The Pedestrian-Only Commercial Streets

The purpose of the pedestrian-only commercial streets, in association with the two urban plazas, is to provide additional retail frontage, complete an interconnected east-west circulation system, and serve as an important pedestrian linkage between the multiple family residential areas and the main commercial street. These streets will have the following features:

- the width of the pedestrian-only commercial streets is 8.00 metres (dedication);
- the buildings will be set back 1.0 metre from the right-of-way;
- a combination of planters and seating with trees is recommended as landscaping;
- arrangement of planters should allow the circulation of an emergency vehicle, if required; and
- decorative pedestrian scale lamp posts and luminaries are to be arranged in an even and continuous way.

A smaller scale urban space is also proposed at the western end of the southern pedestrian-only commercial street. This plaza is a connecting point between the townhouse developments to the northwest and 160 Street. The small plaza is intended to signal the access to the pedestrian street at the bend of the commercial lane.



3. The 84th Avenue Civic/Commercial Street

This commercial street is the east-west portion of the Town Centre area that extends from 159 Street to the Fraser Highway. It will provide a pedestrian friendly environment that supports the neighbourhood commercial and civic activities centered around the Library/Recreation Centre building. Retail commercial activities on 84 Avenue will not be as strong as along the Main Street (160 Street), but will include a variety of civic services, commercial and office spaces.

The streetscape and character envisioned for 84 Avenue is similar to that of 160 Street (i.e., pedestrian scale, distinctive street paving, street furniture, etc.). The streetscape is illustrated on Sketch No. 2. Because 84 Avenue already has a few well-established buildings and an emerging streetscape character, especially at the intersection of 160 Street and 84 Avenue, the design strategy for this street will be oriented to street improvements and reinforcement of the emerging character in conjunction with new developments. Main features of the proposed design for this street are as follows:

The design features proposed for 84 Avenue are:

- wide sidewalks on both sides of the street; special paving patterns and materials similar to that of 160 Street;
- trees planted in grates alongside the on-street parking lanes on both sides of the street and clusters of on-street parking separated by landscaped curb extensions;
- continuous building frontage and wherever possible, buildings should have direct access from the sidewalks;
- off-street parking areas located at the back of the buildings, accessed from a rear lane;
- visual screen and formal definition of street edges for parking lots that are visible from the street; trellis, colonnade structures or low wall and metal fence combined with landscaping are recommended;
- sidewalk pavement to extend across existing driveways into parking areas to identify pedestrian priority over vehicles;
- central median planted with shrubs and high canopy deciduous trees to ensure visual connection between both sides of the street; and
- the significant row of existing trees along the sidewalk on the east side of the Fleetwood Community Centre is to be retained.

4. Intersection of 160 Street and 84 Avenue

The intersection of 160 Street and 84 Avenue is the main focal point of the community south of the Fraser Highway. The existing Fleetwood Community Centre anchors the southwest corner and the Fleetwood Arms Pub is located at the northeast corner. These existing conditions provide the foundation for future development and street improvements at this intersection. Special paving patterns and materials extending across the whole intersection are proposed to mark the significance of this southern anchor area of the Town Centre. Some of the design recommendations to reinforce this urban space are:

- corner cuts in future buildings on the southeast corner to define a corner plaza and open up the intersection;
- improvement of the existing condition at each of the other three corners to reinforce a pedestrian friendly intersection/urban space;
- landscaped buffer and corner feature in front of the existing parking lot at the northwest corner;
- additional landscaping on the Pub site to define a strong and more urban edge for the parking area; and
- revitalization and improvements to the plaza outside the Community Centre to support pedestrian activities.

5. The Fraser Highway Through the Town Centre Area

The Fraser Highway bisects the Fleetwood Town Centre area. To facilitate pedestrian movements across the Highway and link the north and south commercial nodes, the Urban Design Concept proposes to strongly and perceptibly change the appearance and overall character of the Fraser Highway as it crosses through the Town Centre core. Changing the character of this critical portion of the Fraser Highway from 158 Street/Venture Way to the 84 Avenue intersection, would involve changes to the streetscape within the highway right-of-way combined with coordinated and unifying (re)development of private property. Other portions of the Fraser Highway will require special treatment of the edges to buffer land uses (other than highway commercial) from highway noise. Desirable edge conditions for the various sections of the Fraser Highway as it crosses the Town Centre and landmark intersections are as follows:

a) Approaches to the Town Centre

These are those portions of the Fraser Highway that extend from 156 Street to 159 Street/Venture Way on the west, and from 168 Street to 162 Street on the east. Except where the Sports and Leisure Complex and the BC Gas Buildings are located on the north side of the eastern approach to the Town Centre, these portions of the Highway are adjacent to multiple family residential areas.

Some characteristics of the Fraser Highway approaches to the Town Centre are illustrated on Sketch No. 3 (Illustrative Cross-Section A-A) and are described below:

- the edges of these portions of the Highway are intended to provide a landscaped buffer to adjacent multiple family residential areas;
- a 5.5 metre-wide landscaped berm as a buffer between sidewalk and the residential areas;
- a boulevard area with shrubs and trees between the Highway and the sidewalk, on both sides of the Highway; and
- no special pedestrian scale lighting is envisioned along these portions of the Highway.
- A 4.0 metre (13 foot) wide hard surface multi-use path on the north side of the Highway, east of 164 Street (Illustrative Cross-section C-C).

b) Highway Commercial Section of the Fraser Highway

Land uses on both sides of the Highway are currently automobile-oriented highway commercial uses. This portion of the Highway is proposed to be strongly different to announce the presence of the Fleetwood Town Centre to users of the Highway. Sketch No. 3 (Illustrative Cross-Section B-B) shows some details of what is envisaged along Fraser Highway.

The following recommendations are intended to ensure that the desirable special character of this area is achieved:

- Pavement for this portion of the highway should be colour differentiated from the rest of the Fraser Highway (i.e., colour pigment added to the surface mix or the use of concrete);
- establishment of a boulevard strip with shrubs and trees on both sides of the Highway to provide a separation between the roadway and the sidewalk;
- minimize parking areas exposed to views from the Highway by locating buildings close to the front property line and restricting parking on private

property adjacent to the Highway to a single drive aisle and double parking row;

- maximum building setback of 22.0 metres;
- provision of a landscaped band along the Highway frontage to partially screen vehicles in the parking areas;
- provision of pedestrian scale lighting along the sidewalk on both sides of the Highway, placed within the required landscaped buffer area; and
- street lamps alongside the sidewalks to be the same type used along the Town Centre commercial streets.

c) The Linkage to the Sports and Leisure Complex Section of the Fraser Highway

This section extends between 84 Avenue and 164 Street. It includes highway commercial uses on the north and multiple family residential uses on the south side. A multi-use corridor has been established on the south side of the Highway to achieve a strong, visible linkage from the Town Centre to the Sports and Leisure Complex. East of 164 Street, the multi-use corridor continues on the north side of the Highway.

Characteristics of the south side of this portion of the Highway are as follows and are illustrated on Sketch 4 (Illustrative Cross-Section C-C):

- provision of a 4-metre (13 foot) wide hard surfaced multi-use pathway between the Highway and the multiple family residential sites;
- provision of a landscaped berm (containing trees) as a landscaped buffer between the multi-use path and the housing units;
- provision of pedestrian scale lighting along the multi-use path placed on the side of the landscaped mound; and
- lamp poles along the multi-use path to include hardware for the installation of banners.

d) Landmark Intersections

The Fleetwood Town Centre Urban Design Concept defines three significant intersections along the Fraser Highway: at Venture Way, 160 Street and 84 Avenue. Special urban design features are recommended at these intersections, as follows:

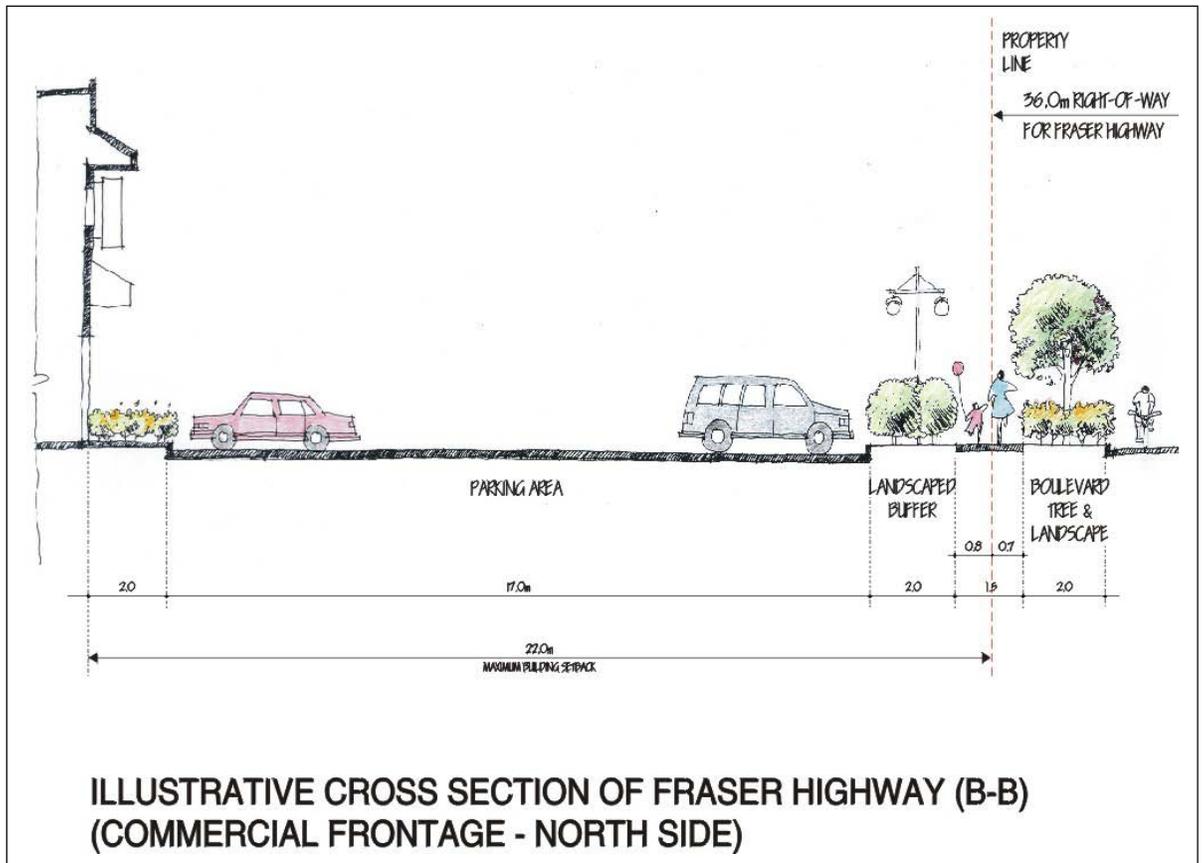
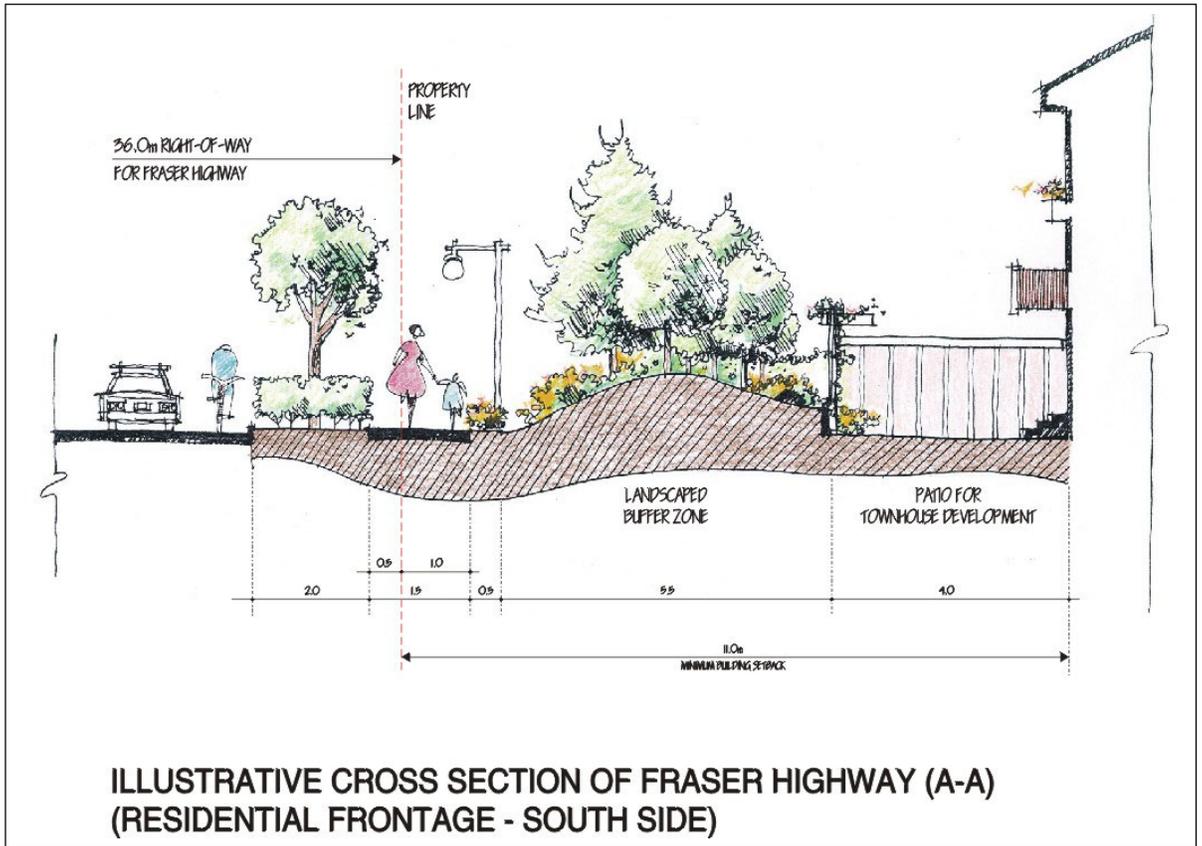
- (1) *Venture Way (159 Street) Intersection - West Gateway:* This is the west gateway into the Town Centre core, and possible design features include:
 - special paving pattern and materials for the whole intersection; and

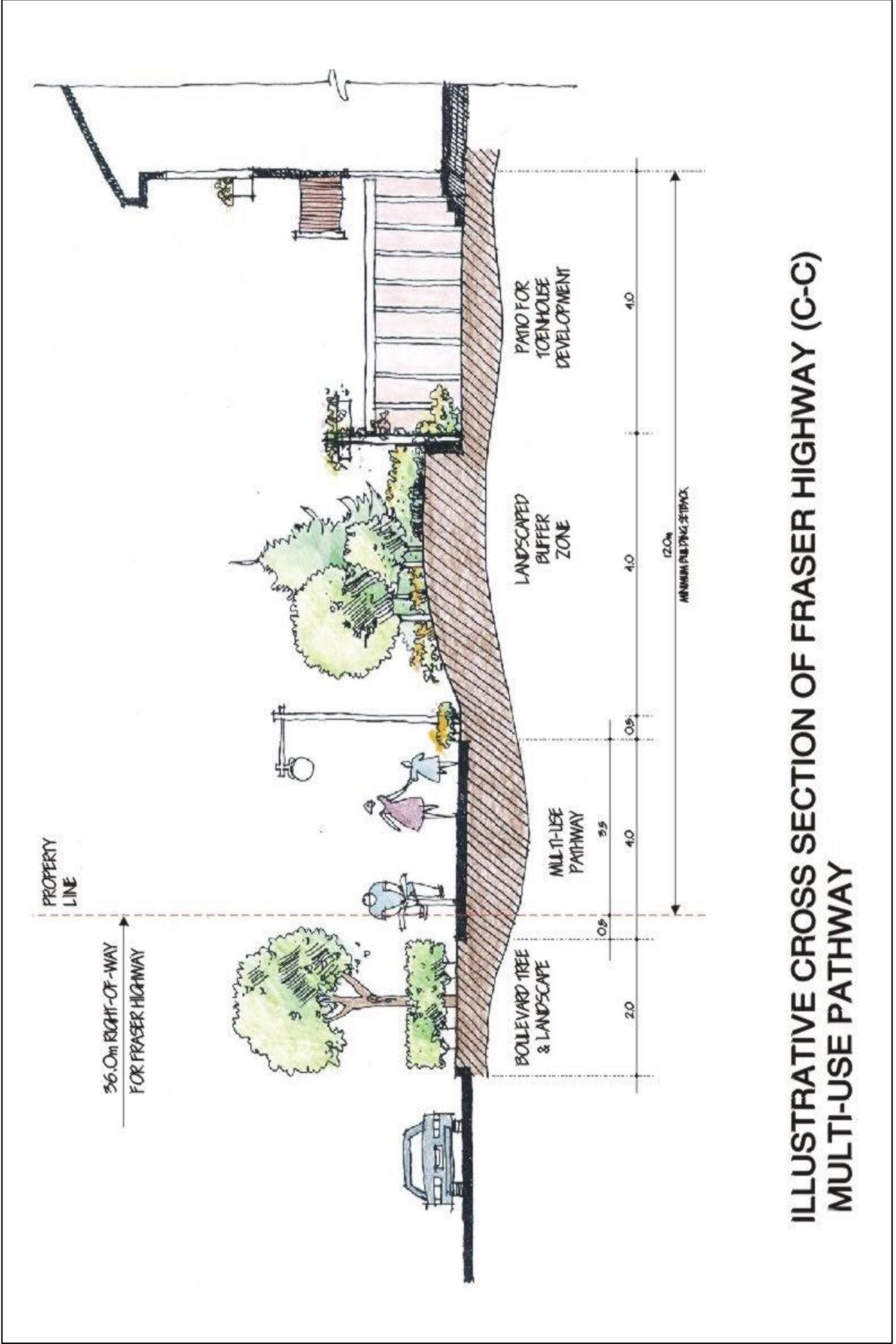
- small urban plazas on four corners and urban landmarks that could include feature landscaping and public art elements.

(2) *160 Street Intersection - Town Centre Core:* This is the most important intersection in the area and is the entrance to the Main Street (both north and south). The following recommendations apply to this intersection:

- the design theme of the main street (160 Street) should extend to the highway frontage so that local character of the Town Centre is present on the Highway;
- pavement of the entire intersection with special paving pattern and materials;
- central median extended close to the crosswalks so that it provides a pedestrian refuge island in the middle of the highway; and
- installation of a major landmark to signal the main entrance point to the Town Centre core and to serve as a reference point to drivers on the Highway.

(3) *84 Avenue Intersection - East Gateway:* The 84 Avenue intersection identifies the eastern boundary of the Town Centre core. Treatment of this intersection should be similar to the Venture Way intersection, and should include special pavement and an urban landmark which could take advantage of the open space proposed at the entrance to the south commercial node.





ILLUSTRATIVE CROSS SECTION OF FRASER HIGHWAY (C-C)
MULTI-USE PATHWAY

IX. FLEETWOOD TOWN CENTRE URBAN DESIGN GUIDELINES

These Design Guidelines will apply to all developments within the boundaries of the Fleetwood Town Centre. The Guidelines have been developed to complement the Development Permit Area Guidelines contained in Surrey's Official Community Plan (OCP) for the purpose of achieving a unique and distinctive character for the Fleetwood Town Centre area.

A. Goals of the Urban Design Guidelines

The following are broad goals associated with the development of the Urban Design Concept for the Fleetwood Town Centre:

1. To establish a commercial and service node located along 160 Street immediately off of the Fraser Highway to serve the needs of and act as the Town Centre focal point for the residents of Fleetwood;
2. To minimize the impact of the Fraser Highway on the Town Center area and integrate the retail/commercial and community services activities that take place on both sides of this major transportation corridor;
3. To recognize the existing highway commercial land uses along the Fraser Highway while also developing a distinctive character for the portion of the Highway that crosses the Town Centre area;
4. To incorporate the Fleetwood Recreation Complex as one of the anchors of the Town Centre;
5. To establish a unique and identifiable local contemporary character for the commercial core of the Town Centre;
6. To develop a recognizably different streetscape character in the Town Center core through the progressive development of coordinated streetscapes and the systematic use of unique street furniture and architectural features that reinforce this character;
7. To facilitate direct pedestrian access to the Town Center core from residential areas on the east and west;
8. To create the conditions for the development of pedestrian-friendly walkable neighbourhoods;
9. To reinforce and enhance the local identity of the Fleetwood Town Centre as a pedestrian-oriented commercial core with a strong human scale and unity of character;
10. To recognize and protect development patterns and the character of existing townhouse and single-family urban subdivisions in planning for future land uses; and
11. To preserve and enhance, where possible, the natural environment and significant stands of vegetation and to incorporate them in the planning of new developments.

B. Objectives of the Urban Design Guidelines

The key objectives of these Guidelines are as follows:

1. Development of pedestrian-friendly residential streets with sidewalks shaded by trees and streetscapes that are not dominated by garages and cars;
2. Development of an aesthetically pleasing community Town Centre by integrating existing and future residential areas and the local commercial core area by formal and coordinated streetscapes;
3. Development of a pedestrian circulation network that links various community activity/destination nodes through a combination of linear parks and on-street/off-street pedestrian/bike corridors that penetrate deep into the single family residential areas around the Town Center core;
4. Development of an identifiable and unique Town Centre area that includes several interconnected, compact, walkable, small residential clusters (of various densities) located in close proximity to the retail/commercial core and community facilities that allow people to shop and play within walking distance of home;
5. Development of the Fleetwood Town Centre Main Street (160 Street) as an identifiable focus for the surrounding community; and
6. Development of a strong public street character through the application of design guidelines for public streets and a variety of boulevard improvement measures, including street trees, pavements and design controls on commercial frontages, front yards and fencing.

C. Design Guidelines for Streets in the Town Centre

1. Street Character

- a) Where possible, the application of the City's Neo-traditional road standards should be used in the residential areas of the Town Centre to accentuate a friendlier, pedestrian-oriented character/atmosphere of the streets;
- b) It is recommended that changes in pavement be used to accentuate pedestrian crossings, major intersections or landmark areas;
- c) To reinforce the pedestrian-oriented quality of the streets and define the dominance of pedestrians over vehicles, decorative pavers (or pavement of contrasting colour and texture) should be used for all on-street parking lanes, and at the driveway entrances to all off-street commercial parking areas;
- d) Curb extensions and chokers are recommended to establish a strong edge definition for areas used for on-street parking;
- e) A low stone or brick faced wall (maximum 0.50 metres high) combined with landscaping is recommended along the street property line of all multiple family residential sites on the main street (160

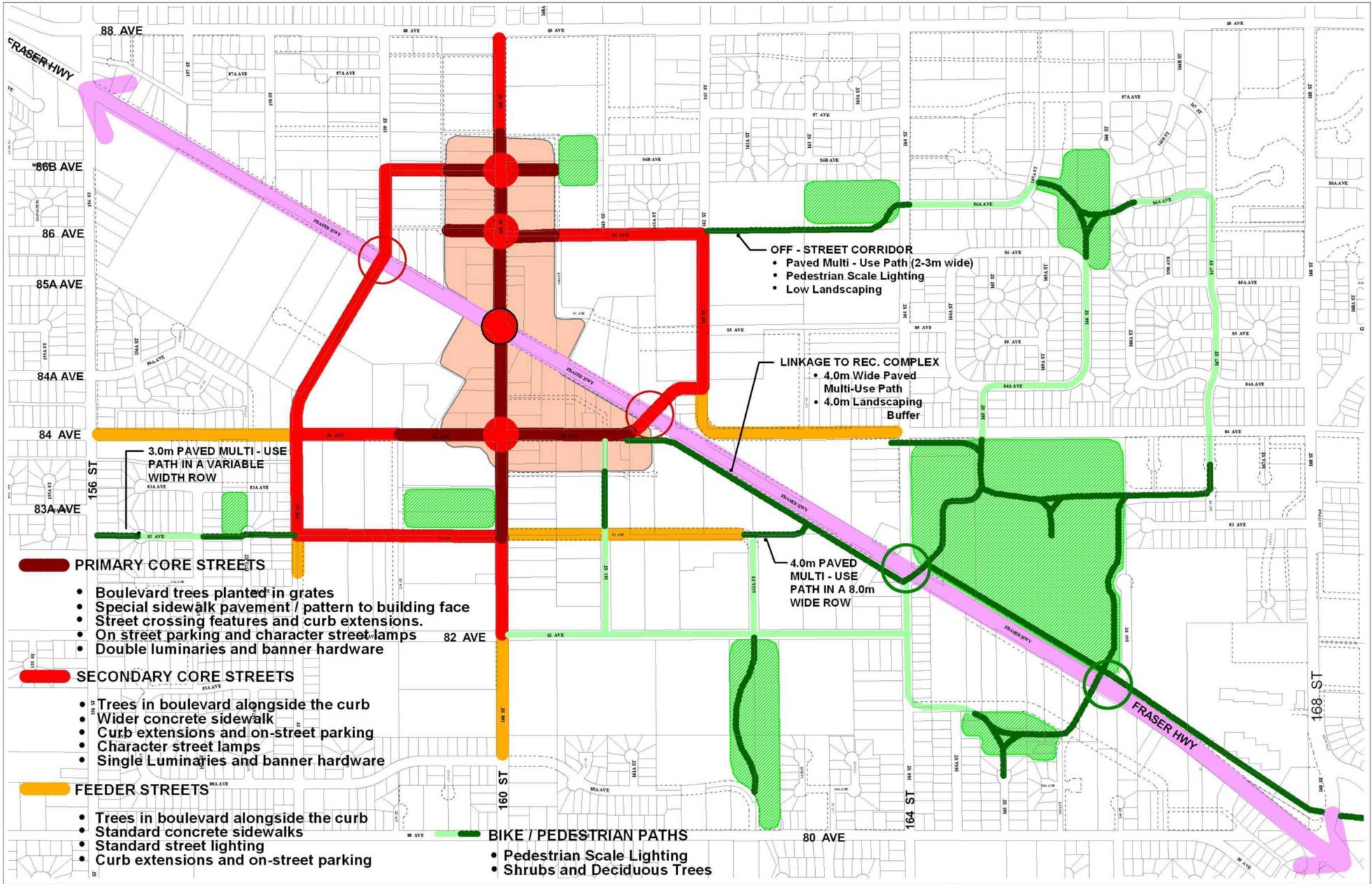
- Street) and on 84 Avenue to establish a formal edge beside the sidewalk and to establish character unity;
- f) Where no rear lane is provided in single family residential areas, garage doors should not dominate the streetscape; garages should be recessed back at least 1.0 metre from the front of the house;
 - g) On single family residential corner lots, garage access is to be provided from the flanking street; and
 - h) Landscaping (shrubs, climbers and trees where possible) is recommended with fences along the lanes.

2. Treatment of Intersections and Parking Areas

- a) Curb extensions (i.e., narrowing of driving lanes) should be provided at all intersections in the Town Centre core area to reduce the crossing distance for pedestrians and to limit vehicle speed;
- b) Traffic calming devices (i.e., small mid-lock curb extensions with landscaping) should be considered for all streets between 159 Street (west), 162 Street (east), 82 Avenue (south) and 88 Avenue (north);
- c) Decorative pavers, stamped concrete or other paving treatment that adds texture and colour differentiation should be used at the major street intersections and pedestrian crossings within this same area; and
- d) Formal tree planting in grates and unique sidewalk pavement (colour, pattern, texture) is required along 160 Street (from 83 Avenue to the commercial lane north of 86B Avenue), along 84 Avenue (from 159 Street to Fraser Highway), and along 86 and 86B Avenues (between the east and west lanes at the back of the commercial areas). (See Map No. 6)

3. Street Lighting

- a) The location of transformer boxes, street lighting and traffic signal poles at or in proximity to any intersection in the Town Centre area should not functionally or visually obstruct the continuity of pedestrian movement from the sidewalk to the street crossing corridor. These should be placed toward the side of the crossing, or against the buildings where possible;
- b) Special character lamp posts with single or double luminaries of a strong contemporary design and character (street light posts and luminaries as already installed along 84 Avenue) are recommended for use in the Fleetwood Town Centre core;
- c) Double luminary character streetlamps, coordinated with trees in grates as indicated above should be provided along the primary core streets and main commercial streets of the Town Centre (See Map No. 6). These lamps should serve pedestrians in the retail/commercial core by being lower, having a gentler glow and being placed at short intervals;
- d) Single luminary character streetlamps, coordinated with trees on the boulevard should be provided along the secondary core streets of the Town Centre core;
- e) Double luminaries character streetlamps should also be provided along the multi-use corridor between the Town Centre (84 Avenue) and the Surrey Sports and Leisure Complex on the Fraser Highway at 162 Street;
- f) Street light posts in the Town Centre core (primary and secondary streets) should include hardware for hanging flower baskets and/or banners and provide pedestrian scale luminaries over the sidewalk;
- g) All other streets in the Town Centre area may be designed using the Surrey standard street lights and sidewalks;
- h) Pedestrian oriented single luminary street lights should be provided along both sides of the Fraser Highway from 84 Avenue to 159/Venture Way to reinforce a different character for the Highway as it crosses the Fleetwood Town Centre core area
- i) Wall mounted lighting fixtures are recommended on all commercial developments on the main street for the purpose of achieving and complementing the required lighting level standard for public streets; and
- j) Surrey Engineering will coordinate implementation of the street lighting concept through the servicing agreement process.



PRIMARY CORE STREETS

- Boulevard trees planted in grates
- Special sidewalk pavement / pattern to building face
- Street crossing features and curb extensions.
- On street parking and character street lamps
- Double luminaries and banner hardware

SECONDARY CORE STREETS

- Trees in boulevard alongside the curb
- Wider concrete sidewalk
- Curb extensions and on-street parking
- Character street lamps
- Single Luminaries and banner hardware

FEEDER STREETS

- Trees in boulevard alongside the curb
- Standard concrete sidewalks
- Standard street lighting
- Curb extensions and on-street parking

BIKE / PEDESTRIAN PATHS

- Pedestrian Scale Lighting
- Shrubs and Deciduous Trees

OFF - STREET CORRIDOR

- Paved Multi - Use Path (2-3m wide)
- Pedestrian Scale Lighting
- Low Landscaping

LINKAGE TO REC. COMPLEX

- 4.0m Wide Paved Multi-Use Path
- 4.0m Landscaping Buffer

4.0m PAVED MULTI - USE PATH IN A 8.0m WIDE ROW

3.0m PAVED MULTI - USE PATH IN A VARIABLE WIDTH ROW



MAP No. 6 Street Features - Circulation Network

FLEETWOOD TOWN CENTRE

Nov. 2, 2000

| | | |
|---|--|------------------------------------|
| Community Amenity Related Landmarks | Gateways to Town Centre and Street Crossing Features | Core Neighbourhood Commercial Area |
| On - Street Bike / Pedestrian Path | Town Centre Urban Landmarks and Street Crossing Features | Inter - Community Bike Corridor |
| Off - Street Bike / Pedestrian Path | Main - Town Centre Landmark (street crossing features) | |
| Parks / Community Facilities and Open Space | | |



NOTE: This plan is conceptual in nature and is only intended to reflect a general pattern of land uses.



D. Design Guidelines for Residential Areas in the Town Centre

1. Streetscape Character in Residential Areas

- a) Landscaping, definition of yard edges, and design of open space areas along public streets should achieve continuity, and help to unify the streetscape;
- b) In single family residential areas, transparent, low fences (1.2 metres high) such as wrought iron, picket, split-rail, lattice, three board or similar styles, in combination with shrubs and hedges are recommended in front yards;
- c) To maintain visual continuity of front yards (and landscaping) along streets, any fences built along the side property lines or alongside a flanking street (generally 1.80 metre high) should not extend within the 7.5 metre area of the required front yard (where the maximum permitted height of fences is 1.2 metres);
- d) The continuity of the public sidewalk pavement across the access driveways should be maintained to establish the dominance of pedestrians over vehicles;
- e) In multiple residential sites, no portion of a fence which is on or closer than 0.90 metres to the property line along a public street should be solid, continuous and without articulation; continuous straight fences should provide a 0.90 metre-wide space between the fence and property line along the street for landscaping on private property;
- f) Recreational vehicle parking, garbage container enclosures, satellite dishes and other service elements should be screened with shrubs and hedges and not be visible from the street;
- g) Arbours, trellises and other creative elements should be considered to define any pedestrian entrance or threshold into multiple residential sites;
- h) Chain link fences are not considered appropriate to the pedestrian friendly character of the Fleetwood Town Centre and should be avoided;
- i) Lattice is recommended for the upper part of any fence along lanes, pedestrian paths and flanking streets;
- j) To enhance the overall quality of the neighbourhood, new developments should retain and incorporate existing clusters of trees; the publication "Saving Native Trees in the Pacific Northwest" (available from the Surrey Parks, Recreation and Culture Department) is recommended as a guideline;

- k) Flowering trees in front yards (especially along the core's secondary streets as identified on Map No. 6 are recommended to add colour and texture to the streetscape;
- l) Some of the flowering trees recommended for yards toward the street are: Stewartia (*Stewartia nonadelpha*), Ivory Silk Tree Lilac (*Syringa reticulata* 'Ivory Silk'), Stag's Horn Sunac (*Rhus typhina*), Magnolia (*Magnolia grandiflora*) and Lavalle Hawthorn (*Crataegus lavallei*);
- m) In residential areas, a large deciduous tree is recommended in the front yard of every single family lot (and one tree for every two units in the case of townhouses); trees should follow the planting pattern and be the same species as the street trees; and
- n) Street planting along the street boulevards other than on primary streets (see Map No. 6) should be determined in consultation with the Parks, Recreation and Culture Department.

2. Character of Residential Developments

A few simple design guidelines for new residential developments in the Town Centre area are proposed to achieve a minimum unity of character. In general, the following character guidelines are applicable to all multiple family residential developments. However it is recognized that certain recommendations are more appropriate for certain types of housing developments than others; site and interface conditions will ultimately determine the applicable guidelines during the development review process.

The preferred housing types in residential areas in or in the proximity of the Town Centre core are street-oriented row-housing and stacked townhouses. Apartment buildings are expected to also reflect a strong street orientation and character.

3. Design Guidelines for Apartment and High Density Townhouse Areas in the Town Centre Core

General design recommendations for developments in this residential area (See Map No. 7) are as follows:

- Reduced front yard setbacks should be considered for apartment and stacked townhouse developments in the Town Centre core; the intent is to reinforce an urban character and reinforce street activity;
- Well-defined individual street-front entries should be provided to ground level units to reinforce a street-oriented character;
- The finished grade of the main floor of homes should be not less than 0.60 metres above the sidewalk level to ensure privacy of lower level units and patios;
- To achieve a strong delineation of public and private space, low stone walls, which may be provided in combination with low transparent metal fences, are recommended;
- Recessed entries should be used to articulate facades and reinforce a unified residential character;
- Windows should be of a simple configuration; large picture windows are not considered appropriate to the desired character for the Town Centre area;
- Rectangular windows with dominant mullions that accentuate vertical lines should be used, and wide trim is recommended around doors and windows;
- The trimming of windows should be consistent throughout all the building elevations;
- Brick, stucco, horizontal siding or a combination of these materials are Light colours should be used for fascia, soffits and trim in contrast with strong colours of siding materials;



- considered satisfactory siding materials; strong colour and contrasting trim and architectural details are preferred;
- Building details and roofing structure (eaves, brackets, etc.) should express a high level of craftsmanship and reflect the local/regional climate;
- Window planters should be considered to create a festive and friendly face for the dwelling units facing the street;
- If fences are unavoidable (particularly within apartment developments), a metal transparent fence (in combination with landscaping and a low stone or brick faced wall as a base) should be used; and
- Front yard fences should not be higher than 1.20 metres.

4. Design Guidelines for Medium Density Townhouses and Single Family Development on Small Lots

- Front setbacks of between 4.0 and 5.0 metres are recommended; it is also strongly recommended that small lots at the interface with single-family residential areas be provided with rear lanes to ensure continuous street frontage;
- Medium density townhouses should have a strong street-oriented character; direct access to the front door from the sidewalk should be provided for all units along a street frontage;
- The design of all townhouse developments should incorporate many of the characteristics of single-family houses (on small lots) to achieve residential character compatibility at higher densities;
- Extended porches and recessed entries should be used to articulate facades and reinforce a unified residential character along the street;
- For the purpose of providing a friendlier, human scale at street level, verandahs and porches with sloping roofs toward the street should be used as dominant features;
- To achieve privacy on porches, verandahs and patios/ decks located toward a public street, the finished grade of dwelling units should not be less than 0.60 metres above the level of the sidewalk;
- Cedar shingles or a similar type of roofing (in terms of colour and texture) or high profile asphalt shingles are preferred to accentuate the dominant single family character of the Fleetwood neighbourhood;
- Metal or red roof tiles are not considered acceptable; roof tiles and duroid may be used only if they resemble cedar shakes in terms of texture, form and colour;

- The use of clapboard siding, scale siding, and board and batten for exterior walls is encouraged; the use of stone as accent material is also recommended;



- Houses on narrow lots should not be less than two storeys in height;
- Flat roofs are not considered appropriate for medium density townhouses and small lot development areas;
- Simple roof forms with strong gable lines, dormers and habitable attics are preferred in small narrow lot house designs;

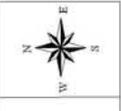
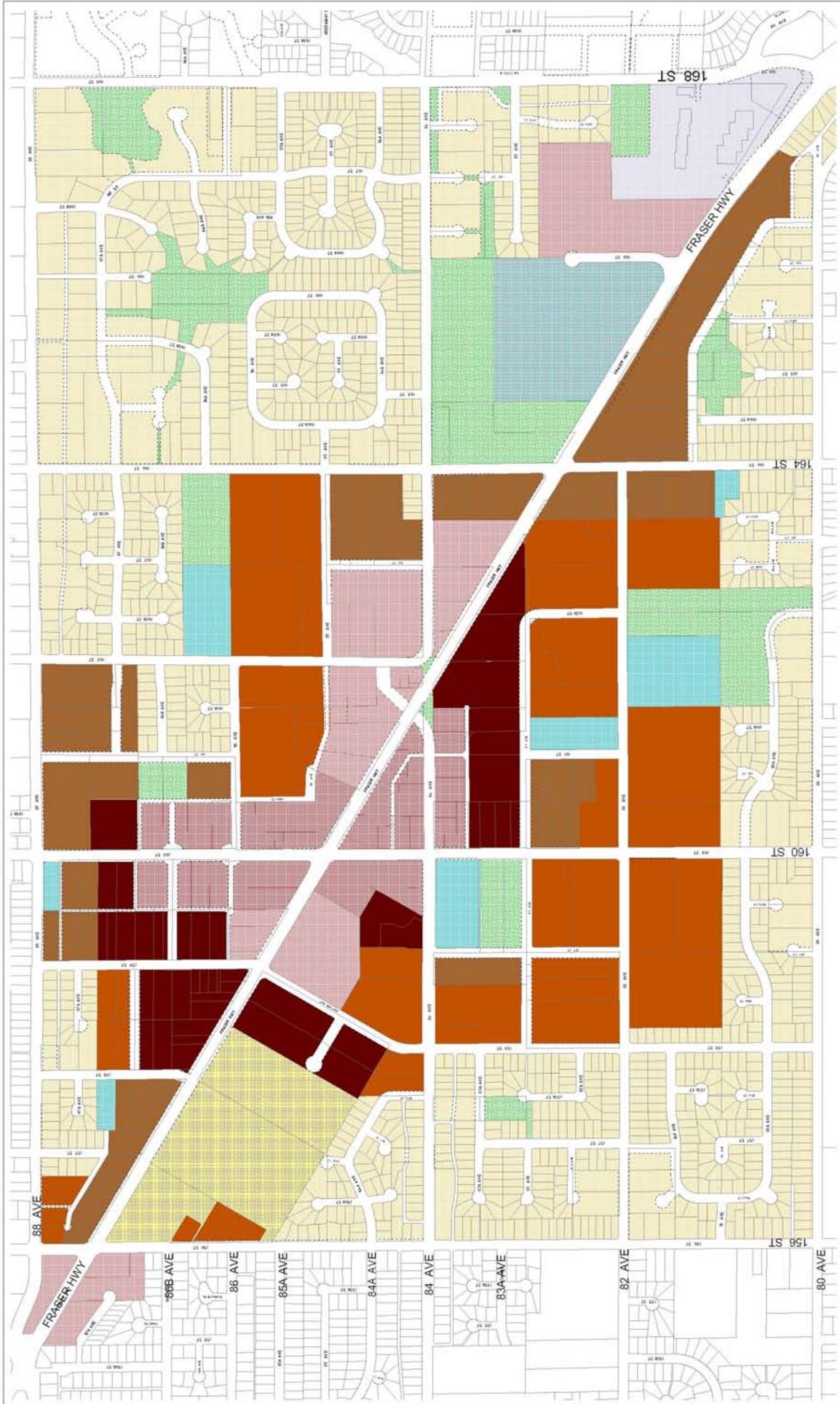
- High pitch and strong roof slopes are recommended (8/12 minimum roof slope);
- Variations in building height, separations, roof lines and setbacks between buildings should be considered to provide visual diversity in the same development;
- Corner units of townhouse clusters, and any housing unit exposed to side views, should provide sufficient architectural detailing in all street-fronting elevations;
- To reflect the dominant single family character Fleetwood, the design of townhouses fronting on the street should incorporate the following architectural features/elements; as a dominant facade component:

- gable roof forms or a dominant gable roof component toward the street with a min. 8/12 slope; gabled dormers, pitched roofs. etc.;
- strong roof overhangs / eave projections;
- louvered ventilation on gables, wind vanes, copulas, shingled or scaled gable end walls, etc.; bay windows;



windows with muntins and mullions; vertical rectangular/square shaped windows; french doors;

- porches, verandahs and colonnades; and
- horizontal siding and wide trim around windows, etc. (stucco to be used in combination with other exterior finishing materials);
- Site layout and design should be based on the principles of defensible space (CPTED) and provide good opportunities for casual surveillance of public areas;
- Landscaping at the perimeter of the site should be designed to reflect the flavour of front yards in single family areas; the use of hedges and shrubs is recommended as an alternative to fences in yards along a public street;
- Fences are not desirable on the front yard area of multiple family residential sites; shrubs and hedges are recommended; if fences are unavoidable, a metal transparent fence (in combination with landscaping and a low stone or brick faced wall as a base) should be used; front yard fences should not be higher than 1.20 metre.



- Apartment / High Density Townhouses
- Medium Density Townhouses
- Low Density Townhouses

FLEETWOOD TOWN CENTRE
 SCALE: 1:2500
 C#3

MAP No. 7 Multiple Family Residential Areas



NOTE: This plan is for informational purposes only and is not intended to be used as a legal document.

Nov. 2, 2000



5. Design Guidelines for Low Density Townhouses

- Strong roof overhangs, projections, etc. should be used to convey the single family character of residential areas in the vicinity of the Town Centre;
- A strong primary roof form (preferably gables) should be provided toward the street, with secondary roofs to visually support the primary roof;
- The minimum roof slope should be 8:12; steeper roofs and attics with dormers are preferred;
- Trellises, arbours and low walls should be considered to define the threshold between public and private spaces; this is recommended at the pedestrian entrances to townhouse sites;
- Where possible, direct pedestrian access from the sidewalk to the main entrance door of each unit along a public road should be provided;
- The design of townhouse clusters along the street should not be repetitive and duplex clusters should avoid the mirror image effect;
- Fences are not desirable in the front yard area of multiple family residential sites; shrubs and hedges are recommended; if fences are unavoidable, a metal transparent fence and brick or stone pilasters (in combination with landscaping) should be used; fences along the street front should not be higher than 1.20 metres;
- Gates at the entrance to a townhouse site are not considered appropriate; the use of strong colour pavement across driveway entrances, minor architectural elements, and landscaping are recommended at the site entrance to define the interface area between private and public areas; and
- Design and material recommendations regarding medium density townhouse developments are also appropriate for low density townhouses to achieve unity of character.



E. Design Guidelines for Commercial Areas

The following guidelines are intended to achieve a harmonious relationship and coordination among commercial buildings and between commercial buildings and the street. Map No. 8 identifies where the commercial areas are located in the context of the Land Use Plan. Map No. 9 identifies the desirable building-street relationships in the various commercial areas in the Fleetwood Town Centre. The following guidelines are intended to achieve a minimum level of unity for commercial developments in the Town Centre area.

1. Overall Streetscape Character

- On-street parking should be provided in small clusters, separated by landscaped curb extensions along 160 Street (in the future); this type of on-street parking should be considered on both sides of 84 Avenue in the South Civic/Commercial Node and along the 86 and 86B Avenue access routes to the main Street (160 Street);
- The use of decorative pavers (or pavement of contrasting colour and texture) is recommended to identify on-street parking lanes; a concrete roll-over curb is suggested at the pavement change line;
- Street trees are to be planted in metal grates along the main retail/commercial areas on 160 Street from 83 Avenue to 86B Avenue and on 84 Avenue between 159 Street and the Fraser Highway;
- Where no on-street parking dedicated lane is possible along 160 Street (north of Fraser Highway), a double row of trees is recommended along the 160 Street retail/commercial area to induce a reduction in speed and establish a stronger pedestrian environment;
- Low shrubs and tree planting on the curb extensions, and pavement treatment at intersections should reinforce the priority of pedestrians over vehicles;
- Sidewalk pattern and type of pavement should extend up to the face of the buildings;
- Trees along the “Primary Streets” and “Secondary Streets” (See Map No. 6) should be selected based on their unique and different colour, shape and foliage, to differentiate the two types of streets at the Town Centre core;
- Trees should be chosen by Parks, Recreation and Culture Department from the list of Replacement Trees recommended for boulevards as per “Schedule K” of the Tree Preservation Bylaw (No. 12880); and
- Trees along any street should include a combination of species in order to provide bio-diversity, and promote tree health by lowering the potential impact of common pests and diseases.

2. Design Guidelines for Retail/Commercial Core Areas

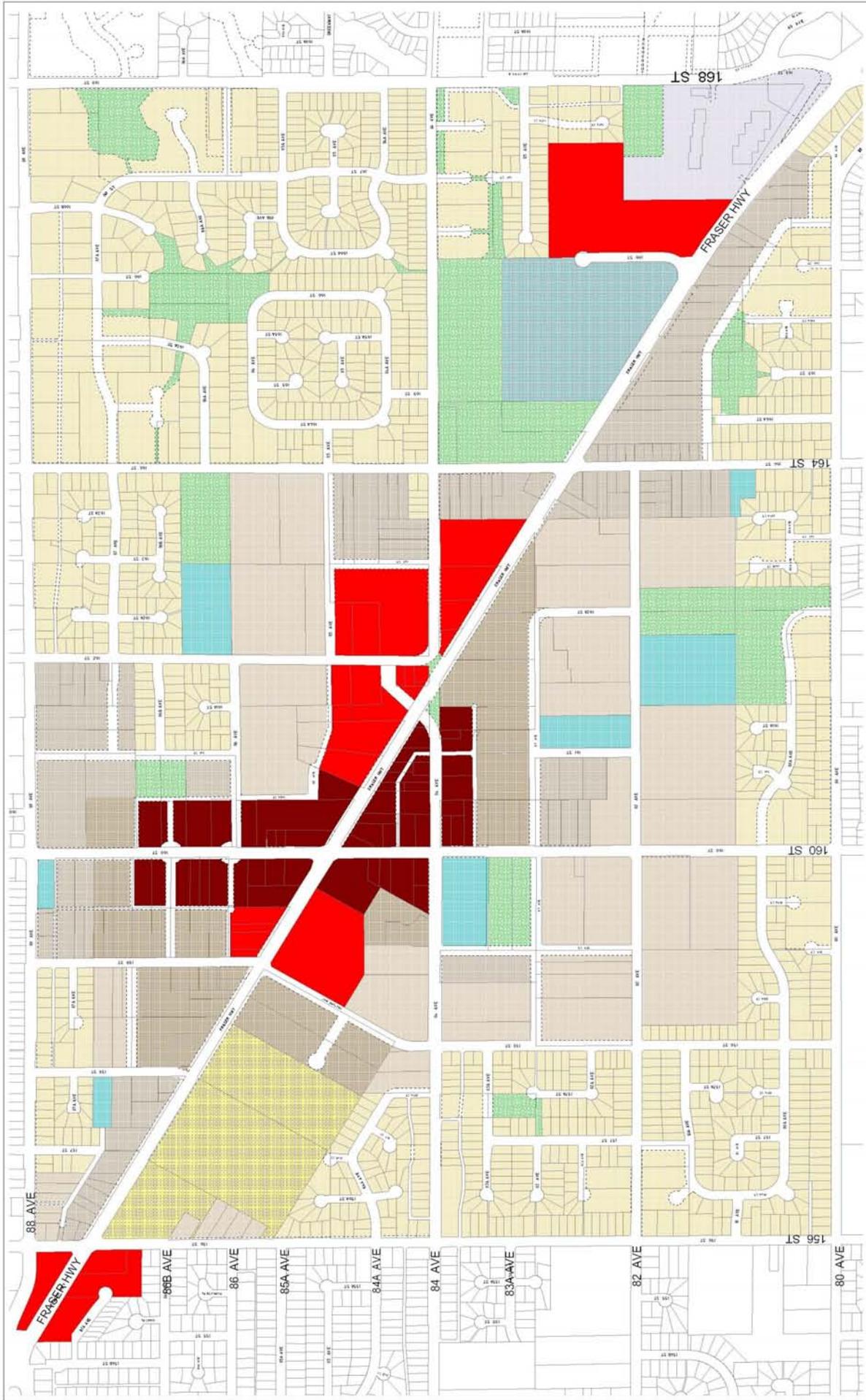
- Commercial building frontage should be continuous along both sides of the street; Sketch 5 is a cross-section of the Main Commercial Street (160 Street) that shows the street - sidewalk relationship;
- Continuity of the commercial frontage is to be achieved by a series of narrow frontage retail bays instead of a large single commercial space;
- The recommended maximum building height on the Main Commercial Street is three to four storeys;
- Developments located at corner sites should have active frontage on both abutting streets; facades should provide the same type of detailing on both streets; these buildings should anchor the corner and act as visual landmarks;
- Interest and articulation of the building mass should be achieved by the creative use of balconies, projections, colours, roof lines and roof slopes, etc.;
- Small front yard setbacks should be provided and the sidewalk pavement should be extended to the building frontage; to achieve a pedestrian-oriented character in commercial areas, a minimum 2.00-metre setback is recommended; changes in setback are recommended to achieve frontage articulation along the length of the street;
- The frontage of the buildings should reflect a strong human, pedestrian scale; direct access to ground level retail uses at short intervals along the street is encouraged;
- Sidewalk surface patterns should extend to the face of the building;
- Tree planting in grates should be provided to accentuate and differentiate the commercial from the residential areas;



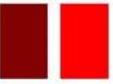
- Buildings should reflect a strong local residential character; roof form, window shapes, storefronts and overall appearance to incorporate characteristic architectural details found in the residential areas;
- If residential uses are provided above the street retail commercial uses, terraces, balconies and/or decks should be located on the levels above the street; upper level(s) should be set back from the ground floor level;
- Desirable materials for retail/commercial buildings include brick and stucco with strong detailing around the storefront (i.e., doors, windows);
- Metal structures and glass should be used for canopies or weather protection features along the street front;
- Bay windows and recessed doors should be incorporated as standard features in commercial storefronts; muntins and wide trims are desirable; and
- Wood for window and door frames should be considered for storefronts; it is recommended that the doors to the retail units fronting the street be made of wood with raised details and inset glass panels.

3. Parking in Commercial Areas

- Short term on-street parking is to be provided along commercial frontages;
- Commercial parking, off-street parking and loading areas are to be located behind the buildings, and screened from direct views from abutting streets and residential areas; access to these parking areas should be provided from the lane;
- A low (maximum 1.00 metre-high) transparent fence combined with landscaping (including trees and/or low raised planters) should be used to screen parking areas from the lane;
- Buildings should provide corridors that provide access to the front of the building from the parking area at the back;
- Decorative pavers are to be used to differentiate parking areas from driving aisles and pedestrian routes to building entrances; and
- Pedestrian scale decorative lamp posts/luminaries should be provided in parking areas and along lanes at the back of commercial areas.



Community Commercial
 Highway Commercial



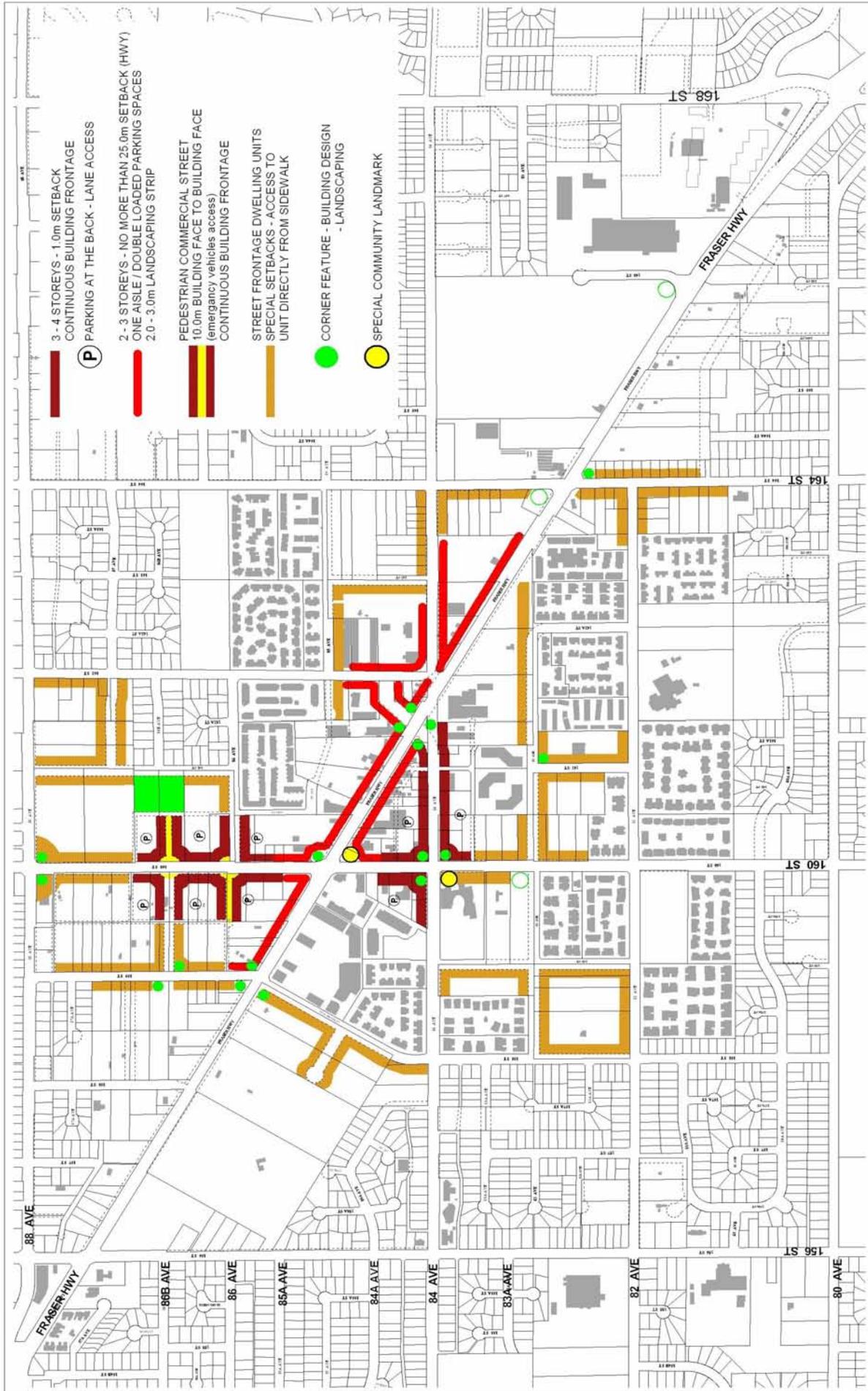
FLEETWOOD TOWN CENTRE
 SCALE: 1:2500 C#3

MAP No. 8 Commercial Areas

Nov. 2, 2000



NOTE: This map is intended to inform and is not intended to be used as a legal document.



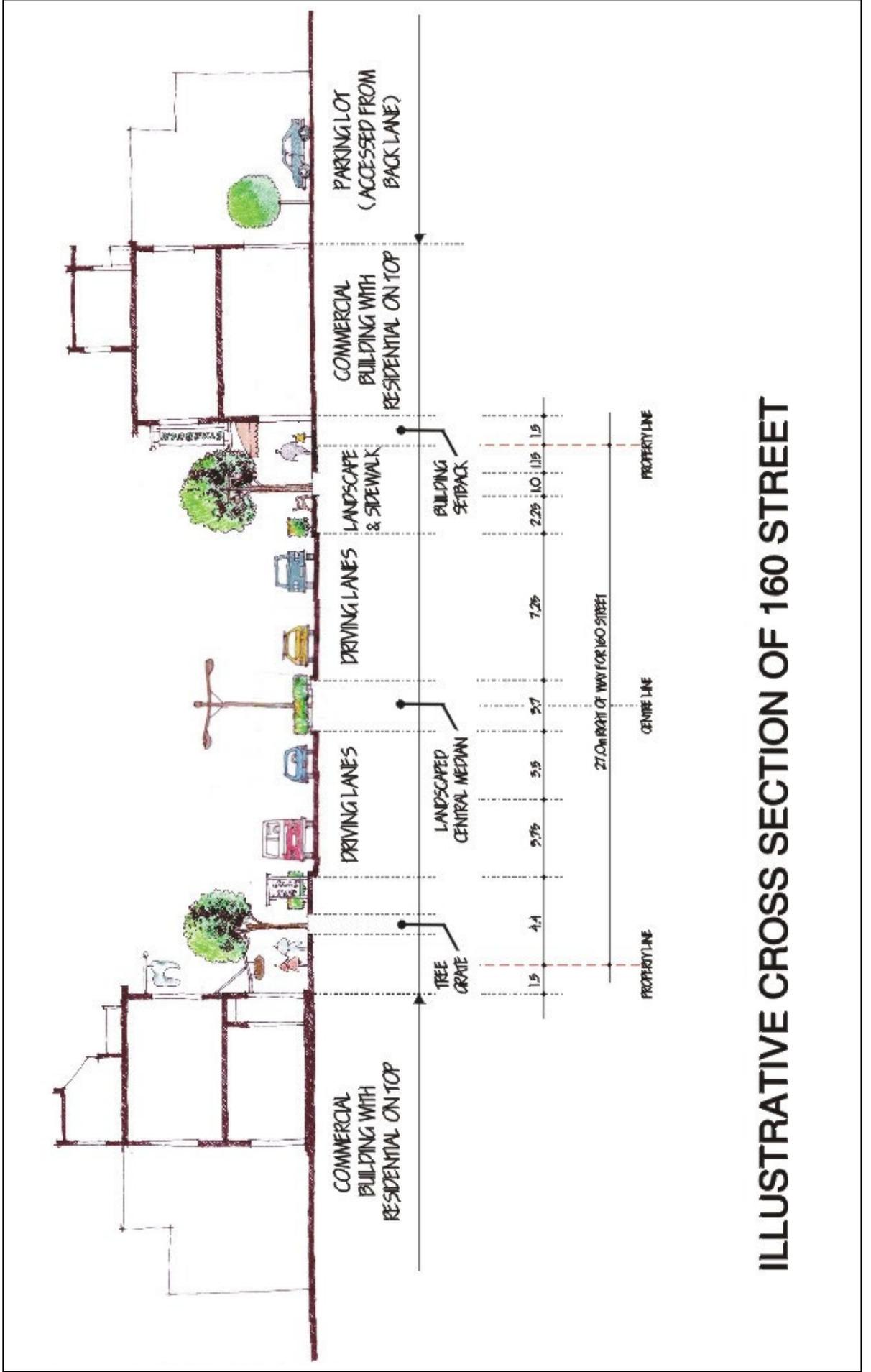
MAP No. 9 Building Frontage Relationship to Street

FLEETWOOD TOWN CENTRE

Nov. 2, 2000 SCALE: 1:2500 C#3



NOTE: This plan is developed on a base map and is intended to reflect a general pattern of land use and street character.



ILLUSTRATIVE CROSS SECTION OF 160 STREET

4. Canopies and Signs

- The use of canopies and awnings over the sidewalk is encouraged for commercial development on the Main Street (160 Street) to provide continuous weather protection along the building frontage;
- Glass and metal are recommended as primary materials for canopies and weather protection elements;
- A 45-degree inclination slope is recommended for canopies; compatibility and co-ordination of canopies is encouraged;
- If not made out of glass, canopies should be of a flat, solid colour and should be coordinated with the colour scheme of the building;
- Sign size, location and information provided on signs should be designed and oriented to pedestrians and relate to the scale and character of the commercial area;
- Fascia signs and projecting signs are not considered appropriate to the more contemporary architectural character of the Town Centre area; and
- Free-standing pylon signs are not considered appropriate for the Town Centre core commercial area.

5. Design Guidelines for Highway Commercial Areas

- Two storey buildings are recommended along the highway frontage;
- Buildings should be located with their longer side parallel to the Highway to define a continuous building line on both sides of this major regional corridor;
- Buildings should not be located more than 22.0 metres from the front property line on the Highway;
- Direct access to a commercial site from the Fraser Highway should be avoided (access from the back should be used wherever possible); if this is not possible shared access should be provided between properties;
- Site planning should make provision for parking aisles extending between parking areas on adjoining sites to minimize the number of access points from the Highway;
- A 2-metre (6.5 ft.) landscaped strip should be provided between the sidewalk and any proposed parking area along the Highway; pedestrian scale streetlights should be provided in this landscaped area; and
- Any free-standing sign along the highway frontage should be architecturally coordinated with the design of the building on the same site and should be included as part of the development proposal.

Corporate NO: C016
Report COUNCIL DATE: December 4, 2000

COUNCIL-IN-COMMITTEE

TO: **Mayor & Council** DATE: **November 30, 2000**
FROM: **General Manager, Planning and Development** FILE: **2350-0013**
SUBJECT: **Fleetwood Town Centre Land Use Plan and Urban Design Concept (Stage 2 – Final Report)**

RECOMMENDATION

It is recommended that Council:

1. Approve the attached Fleetwood Town Centre Land Use Plan and Urban Design Concept as a means for directing and managing the development and design of the Fleetwood Town Centre;
2. Amend the Fleetwood Local Area Plan by including the attached Fleetwood Town Centre Land Use Plan and Urban Design Concept;
3. Authorize staff to draft an amendment to Official Community Plan By-law No. 129000 to change the OCP land use designations required to implement the Fleetwood Town Centre Land Use Plan; and
4. Authorize staff to draft an amendment to Zoning By-law No. 12000, as amended, to include an amenity contribution provision based upon the density bonus concept for the Fleetwood Town Centre area.

INTENT

The intent of this report is to provide a summary of the planning process and a brief overview of the attached “Fleetwood Town Centre Land Use Plan and Urban Design Concept”, and to seek Council’s final approval of the Plan and Urban Design Concept (Stage 2).

BACKGROUND

On March 22, 1999, Council granted approval in principle to the proposed Land Use Plan for the Fleetwood Town Centre (Stage 1) and authorized staff to commence the preparation of Stage 2, including a comprehensive financial plan, urban design guidelines and a pedestrian/bicycle network plan. The Stage 2 component of the Plan is now complete. A report from the Engineering Department on the detailed servicing and funding strategy is to be considered in conjunction with this report.

DISCUSSION

Study Area

The Fleetwood Town Centre planning area is focused on the historic centre of Fleetwood at the intersection of the Fraser Highway and 160 Street and 84 Avenue. The planning area is bounded on the north by 88 Avenue, the south by 80 Avenue, the west by 156 Street and the east by 168 Street.

Objectives of the Study

The objectives of the Fleetwood Town Centre planning study were:

1. To update the Fleetwood Town Centre plan due to the changes that have occurred in the Fleetwood area;
2. To review the adequacy of land designated for Town Centre uses in the long term and the nature and form of development desired by the residents and property owners;
3. To prepare a servicing and financial strategy to support the Land Use Plan; and
4. To develop an urban design concept to create cohesion, a sense of community identity and a unique character for the Town Centre area.

Overview of the Planning Process and Public Consultation

The planning process commenced in late 1997 with the engagement of a consultant (Plancon Consultants Inc.) to review the current land uses, initiate the public consultation process and develop a generalized land use plan for the Fleetwood Town Centre. This work, which was concluded in March 1998, provided the basis for the initial (Stage 1) Land Use Plan.

The Planning & Development Department established an in-house Steering Committee and undertook an extensive process of public consultation during the preparation of the Fleetwood Town Centre Plan. Consultation with the community included three Public Information Meetings, open houses, informal and formal surveys of the residents and on-going discussions with residents, property owners, merchants, the Fleetwood Community Association and the development industry. The final public meeting was held on September 19, 2000 at which about 200 people attended. While there is wide community support for the Plan, several concerns were raised at the meeting and passed on through the questionnaires. These concerns have been resolved and incorporated into the final Land Use Plan.

Overview of the Land Use Plan

The Fleetwood Town Centre Land Use Plan is shown on Map No. 2 in the attached document. The Town Centre is expected to accommodate about 157,000 square metres (1.7 million square feet) of commercial/office space at full build-out which is about 80,000 square metres (860,000 square feet) more than what exists now. There will also be an additional 5,000 dwelling units constructed (a total of 7,600 at full build out). The population of the Town Centre at build-out is expected to be about 20,000 and the entire Fleetwood community is expected to have a population of 60,000 at saturation.

1) Commercial

An easily accessible commercial core is proposed to be located along 160 Street from 84 Avenue to 87 Avenue which will serve as a main commercial street for the entire Fleetwood community. A secondary smaller community services node is proposed south of the Fraser Highway at the intersection of 160 Street and 84 Avenue (in conjunction with the Fleetwood Library and Community Centre and eastward toward the Fraser Highway). Pedestrian-oriented commercial uses will be developed along 160 Street: the northern area is expected to accommodate shopping and retail, and the southern area is intended to accommodate community services such as medical offices and civic facilities.

To respond to the desire of Fleetwood residents for a compact, pedestrian-oriented community, the focus of the Fleetwood Town Centre is proposed to be shifted away from highway commercial uses by containing the spread of auto-oriented commercial activities along the Fraser Highway and by expanding the opportunity for community commercial uses located within walking distance of residential areas.

2) Residential

The Land Use Plan proposes to protect existing residential areas by providing appropriate transitions between residential and commercial and between different residential densities. It also provides for interconnectivity between the residential areas and the commercial core area and recommends a general building height restriction (4 storeys or 15 metres/50 feet) to foster a more pedestrian scale environment. To support viable commercial activities in the core commercial area, higher density residential development is proposed in close proximity to the commercial streets.

3) Industrial

There are several properties within the Town Centre in the vicinity of 162 Street and 84 Avenue that are designated Industrial in the Official Community Plan and which have been developed with highway commercial uses (which is a permitted land use in the Industrial OCP designation). These properties are identified in the Land Use Plan as “highway commercial” and are expected to remain as such.

In the eastern area of the Town Centre, the BC Gas site is designated Industrial in the Official Community Plan. It contains an office building and some storage buildings. The

Town Centre Land Use Plan designates this site for industrial uses which may include business park, light impact industrial or highway commercial uses.

4) *Civic Facilities*

The Surrey Sports and Leisure Complex site is recognized as a destination in the community and is accommodated through an institutional/commercial designation. The Fleetwood Library and Community Centre/Park area is also recognized as a destination and the “civic” centre of Fleetwood. The existing schools in the Fleetwood Town Centre are able to accommodate the projected school enrolment resulting from new residential development in the area.

5) *Schools, Parks and Circulation*

There are three neighbourhood school/park sites within the Fleetwood Town Centre along with a sizable neighbourhood park in the north-east residential area, a large park area adjacent to the Leisure Centre on the Fraser Highway, and other small neighbourhood parks, open spaces and greenways. There will also be several urban plazas along the 160 Street shopping street.

The pedestrian and bicycle network (see Map No. 6 in the attached document) proposed for the Fleetwood Town Centre is based upon a series of inter-connected routes (on and off-street) that provide easy and safe access to the commercial core, the school/park sites and the Surrey Sports and Leisure Complex.

Several minor changes to the Stage 1 Land Use Plan have been made as a result of localized conditions and constraints, technical reviews and public input. The locations of these changes are shown on the map in Appendix A. A description of each change is outlined below:

Description of Modifications to Stage 1 Land Use Plan

- (A) Changed from townhouse designation to park designation to respond to the need for a consolidated community park site for Fleetwood.
- (B) Changed from townhouse designation to park designation to establish a neighbourhood park in conjunction with the school.
- (C) Changed from townhouse designation to park designation to provide a neighbourhood park close to the higher residential densities in the Town Centre core.
- (D) Intersection realigned at 84 Avenue/Fraser Highway to ensure that residential areas are not impacted by high volumes of traffic.
- (E) East-west pedestrian connection established to provide access to the Town Centre core from the east residential areas (north side of Fraser Highway).
- (F) Road moved slightly south to retain an interconnected system of streets and lanes in the Town Centre core and provide access from the west residential areas.
- (G) Road access provided to multiple residential area along the Fraser Highway rather than a frontage road (see below).

Outstanding Concern

It is noted that there has been a community concern about the proposed change (G) above. While the land use designations have not changed, existing residents on 164A and 165 Street are concerned about the potential for increased traffic on their streets if the existing culs-de-sac are opened and connected to a street at the rear of the multiple residential sites.

The Stage 1 land use plan indicated that the multiple residential area was to be accessed by a frontage road off the Fraser Highway. Upon a more detailed review of the circulation system, it was determined that an internal road separating the multiple residential and the single family residential would better disperse traffic, allow better access to the multiple residential area, allow for more efficient development patterns for the future single family subdivisions and provide public access to the existing park (which is currently only accessed from the culs-de-sac). In addition, it is anticipated that 164A and 165 Streets will not experience a significant increase in traffic because most trips from the multiple residential area will be toward the Fraser Highway rather than southward.

However, in recognition of the residents concerns, the plan has been modified to eliminate connections from the existing north-south streets (i.e. 164A and 165 Streets). Pedestrian connections between these culs-de-sac and the new road would still be appropriate to the new east-west road. In addition, the location of the new road may move slightly to the north depending on the nature and timing of development in the multiple residential area and on the needs of the future single family subdivisions for access. These issues can be dealt with in conjunction with rezoning applications, and specifically by the time the first rezoning application is completed for multiple residential along the Fraser Highway.

The Urban Design Concept and Design Guidelines

The proposed urban structure for the Fleetwood Town Centre recognizes the need to define a strong node that serves the community and that is accessible to local residents on both sides of the Fraser Highway by car, bicycle and by foot. The Plan also recognizes the presence of highway commercial uses along the Fraser Highway, and the community services node defined by the old Fleetwood Community Hall, the Library and the Senior's/Recreation Centre at 160 Street and 84 Avenue. The Urban Design Concept establishes 160 Street as the north-south retail/commercial spine that serves and links the residential areas on both sides of the Fraser Highway.

The Concept proposes the following basic components:

- 1) *Two commercial nodes (north and south of the Fraser Highway):* The north commercial shopping street will have a strong pedestrian oriented urban character with wide sidewalks and continuous building frontage. Local retail activities and mixed-use commercial/residential developments are envisaged in this area along with two small plazas at the intersections of 160 Street with 86 and 87 Avenues. The south commercial node is focused on 84 Avenue and the Library/Recreation Centre at the 160 Street intersection. This node combines office/retail uses along both sides of the street. Community service uses will be focused on the existing community facilities centered

on the Library. Office/retail uses with commercial frontages are proposed to extend eastward along 84 Avenue to the intersection with Fraser Highway.

- 2) *Transition commercial area along the Town Centre Main Street (160 Street):* This is a transition area that extends across the Fraser Highway, along 160 Street, between the two commercial nodes referenced above. It includes a combination of existing highway commercial and retail commercial types of development that have parking areas rather than building facades toward the street.
- 3) *Highway commercial frontage:* This is the existing strip of automobile-oriented development along both sides of the Fraser Highway between Venture Way and 162 Street. Although the present land uses will likely continue to exist, it is intended that the quality of the built environment be modified to relate to the character of the Town Centre. Reducing the extent of parking areas exposed to views from the Fraser Highway and locating buildings closer to the Highway will permit a better definition of the highway edges and a more urban character for these commercial land uses.
- 4) *The Surrey Sports and Leisure Complex:* This Complex is incorporated into the Town Centre by a short multi-use corridor linkage that runs along the south side of the Fraser Highway between the 84 Avenue intersection with Fraser Highway and 162 Street. This Complex will be a strong anchor on the east end of the Town Centre core.
- 5) *The Multiple Family Residential areas surrounding the central core:* These more compact residential areas in close proximity to the Town Centre's local commercial spine will help to reinforce the pedestrian-oriented and urban character of the area. Mixed-use developments which combine retail/commercial and residential uses might also be considered for this area since they would help to provide nighttime activity on the Town Centre's main commercial street (160 Street).

Map No. 5 in the attached document provides a visual overview of the components of the Urban Design Concept for the Fleetwood Town Centre.

Implementation

1) *Official Community Plan Land Use Designations*

Amendments to the current land use designations in the Official Community Plan (OCP) are required to implement the Fleetwood Town Centre Land Use Plan. The existing and proposed OCP land use designations are shown on Map No. 10 in the attached document. Due to the significantly large areas requiring OCP amendments, it is recommended that the amendments to the OCP be implemented now by a City-initiated OCP amendment by-law. Generally, the amendments involve redesignating lands along 160 Street from Urban to Town Centre, and lands adjacent to the commercial areas and along Fraser Highway from Urban to Multiple Residential.

2) *Amenity Requirements*

To address the amenity needs associated with new growth in the Fleetwood Town Centre, new development will be required to contribute \$1,055.88 per dwelling unit or \$1,094.10 per commercial acre. These monies will be used to offset the costs of providing new police and fire protection services, library materials and the development of new parks and open space. The collection of amenity contributions is authorized through an amendment to the Zoning By-law to add the Fleetwood Town Centre as an area where contributions are required.

3) *Street Improvements*

Most of the street improvements and beautification schemes proposed in the Urban Design Concept apply to 160 Street and to the Fraser Highway. Both 160 Street and the Fraser Highway are classified as arterial roads and their widening/construction will be funded through DCCs (the Fraser Highway may be partially funded by Translink). The 160 Street cross-section through the Town Centre includes wide sidewalks, street trees in grates and other features which will be incorporated into the design of 160 Street when it is upgraded. It is also proposed that the character of the Fraser Highway be changed as it crosses through the Town Centre. This too will be incorporated into the design and construction when Fraser Highway is ultimately widened. Other improvements (i.e., a portion of the small plaza and pedestrian streets) will occur as commercial development proceeds along 160 Street.

CONCLUSION

The Fleetwood Town Centre Land Use Plan and Urban Design Concept responds to the objectives of the Fleetwood community and establishes a new vision and co-ordinated approach toward planning and development initiatives in the Town Centre. It is a comprehensive planning and design document which establishes land use patterns, vehicular and pedestrian circulation concepts and open space systems. The urban design component of the Plan provides design direction to achieve the community-wide objectives derived through the planning process. The entire Plan has been developed with extensive consultation with the public and it is widely supported by the community. It is therefore recommended that, in conjunction with approving the servicing and funding strategy, Council approve the Plan and authorize staff to proceed with actions to implement the Plan.

(Signed) Murray D. Dinwoodie
General Manager, Planning and Development

WW/bea

c.c.: - General Manager, Engineering
- General Manager, Parks, Recreation & Culture

Attachments:

- Fleetwood Town Centre Land Use Plan and Urban Design Concept
- Appendix I - Modifications to Stage 1 Land Use Plan

APPENDIX A

LOCATION OF MODIFICATIONS TO THE STAGE 1 LAND USE PLAN

